



HEARTLAND CORRIDOR STRATEGIC

ECONOMIC DEVELOPMENT
AND LAND USE PLAN

Tippecanoe County, Indiana | 2016

FEBRUARY 1, 2016

Tippecanoe County Commissioners

Tracy Brown
Dave Byers
Tom Murtaugh

Tippecanoe County Area Plan Commission Staff

Sallie Fahey
Larry Aukerman
John Burns
Linda Eastman
Don Lamb
Kathleen Lind
Ryan O’Gara
Doug Poad
Timothy Stroshine
John Thomas
Linda Underwood

Tippecanoe County, Indiana

20 North Third Street
Lafayette, IN 47901
<http://www.tippecanoe.in.gov/>



Steering Committee

Sallie Fahey – APC Executive Director
Tom Murtaugh – County Commissioner
Paula Bennett – Tippecanoe County Board of Commissioners Assistant
Pat Brettnacher – DogWood Kennel
Jeff Brown – Agricultural Property Owner
Tracy Brown – County Commissioner
Dennis Carson – Lafayette Redevelopment Director
Mark DeYoung – Residential Neighborhood Representative
James Gross – Milestone Contractor
Jody Hamilton – Greater Lafayette Commerce
Scott Hanback – Tippecanoe School Corporation
Chico Hatke – Residential Neighborhood Representative
Nate Hoffman – Americus Group
Barb Knochel – Washington Township
Susan Linson – RoadWorks Manufacturing
Ryan O’Gara – APC Assistant Director
Bill Price – Redevelopment Commission
Julie Roush – Fairfield Township
Scott Walker – Greater Lafayette Commerce
Laura Walls - Carroll County Economic Development

Consultant

American Structurepoint
7260 Shadeland Station
Indianapolis, Indiana 46256
317.547.5580
www.structurepoint.com



This plan was adopted by the Tippecanoe County Commissioners and the Lafayette City Council on February 1, 2016.

(THIS PAGE INTENTIONALLY LEFT BLANK)

Introduction

- 8** What is the Heartland Corridor Strategic Plan?
- 9** Background of the Plan
- 10** Purpose and Use of the Plan
- 11** Planning Process

Existing Conditions

- 14** Study Area
- 16** Land Use
- 19** Zoning
- 19** Transportation and Circulation Framework
- 22** Infrastructure Framework

Strategic Plan Foundation

- 26** Vision and Goals
- 27** Strategic Plan Principles

Recommendations

- 32** Future Land Use Plan
- 32** Description of Land Use Components
- 38** Circulation Infrastructure
- 40** Utility Infrastructure
- 53** Economic Development Tools
- 54** Site and Design Standards

Implementation Tools

- 58** Critical Path Strategies

Appendices

- Appendix A - Public Meeting Materials
- Appendix B - Large Versions of Maps
- Appendix C - Infrastructure Cost
Estimates
- Appendix D - Resolutions

(THIS PAGE INTENTIONALLY LEFT BLANK)

INTRODUCTION

Background of the Plan
Purpose and Use of the Plan
Planning Process

What is the Heartland Corridor Strategic Plan?

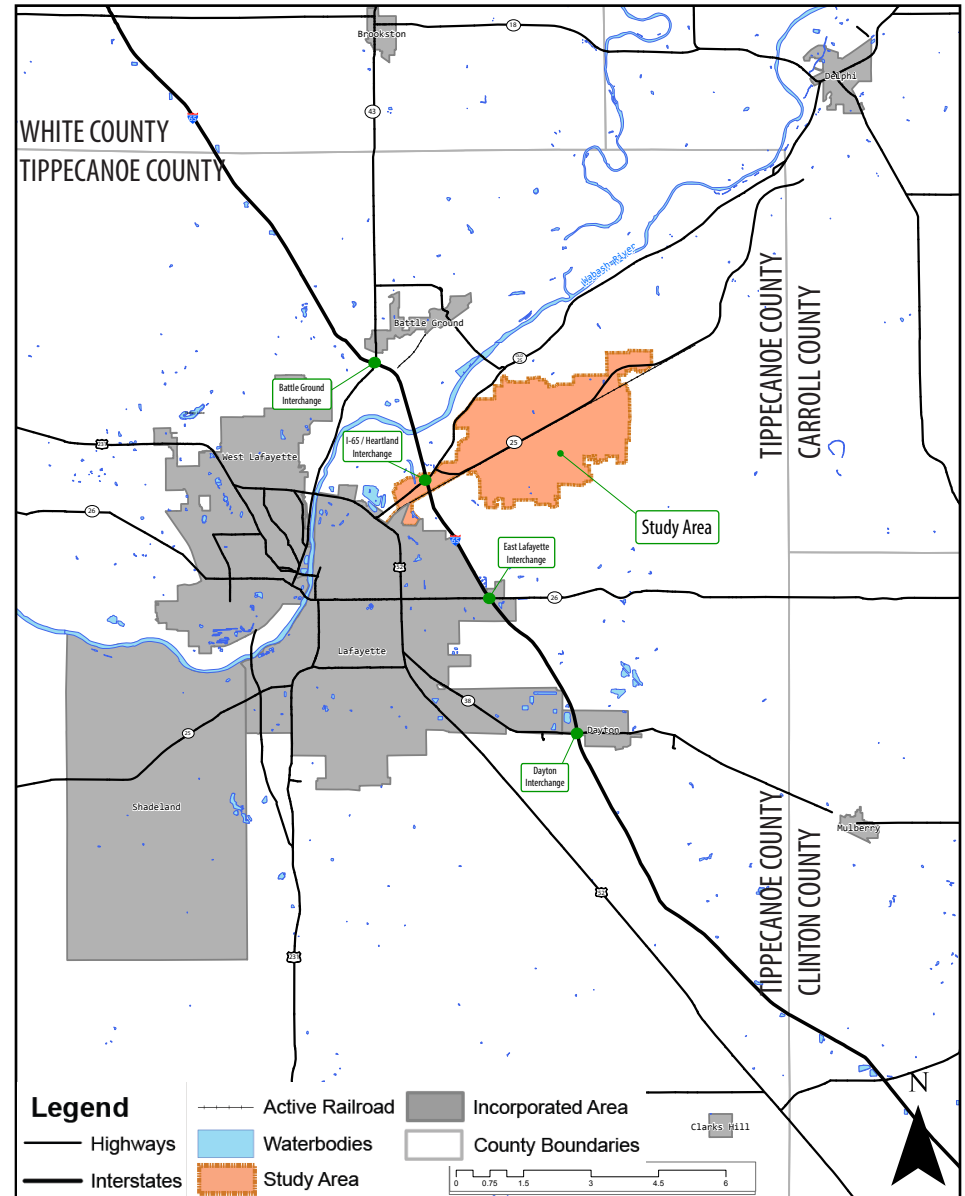
This document presents the Heartland Corridor Strategic Plan. The plan outlines the county's short- to long-term plan for improvement, development, and growth for the study area located in northeast Tippecanoe County and portions of Lafayette, centered on the area between the intersection of Interstate 65 and Hoosier Heartland Highway Corridor (State Road 25) and the eastern county line near Buck Creek in the location of the Lafayette/Tippecanoe County tax increment financing districts, known as the Tippecanoe County Heartland Economic Development Area.

The plan establishes a foundation for future decision-making regarding land use and infrastructure development and transportation circulation. It is intended to guide appropriate, market-viable and context sensitive development in the study area. The recent completion of the Hoosier Heartland Highway Corridor improves access to Interstate 65 and changes the characteristics of the study area. This area of the county now has increased access to the interstate system and the regional transportation network which increases the interest and opportunity to develop the properties for a variety of uses.

The plan is an adaptive policy guide intended to be flexible and adaptive over time. While the plan outlines specific recommendations for specific areas, the plan also sets a foundation to guide unexpected, unforeseen opportunities, as well as changes in the area and regional forces. This plan allows for the adaptation and adjustment as conditions and opportunities change within the study area. It can also accommodate opportunities that exceed the expectations and aspire to implement the community vision for this area as needed. It is a document that can be used to provide opportunity for community members to be proactive in the community.

Policy Summary: This plan is not a rezoning document, and the zoning classifications within the study area are not changed because of the recommendations of this plan. Any future zoning changes within this study area, whether initiated by the county or by a private land owner, must follow standard Indiana law and the county process for zoning and ordinance changes. This plan

Regional Map



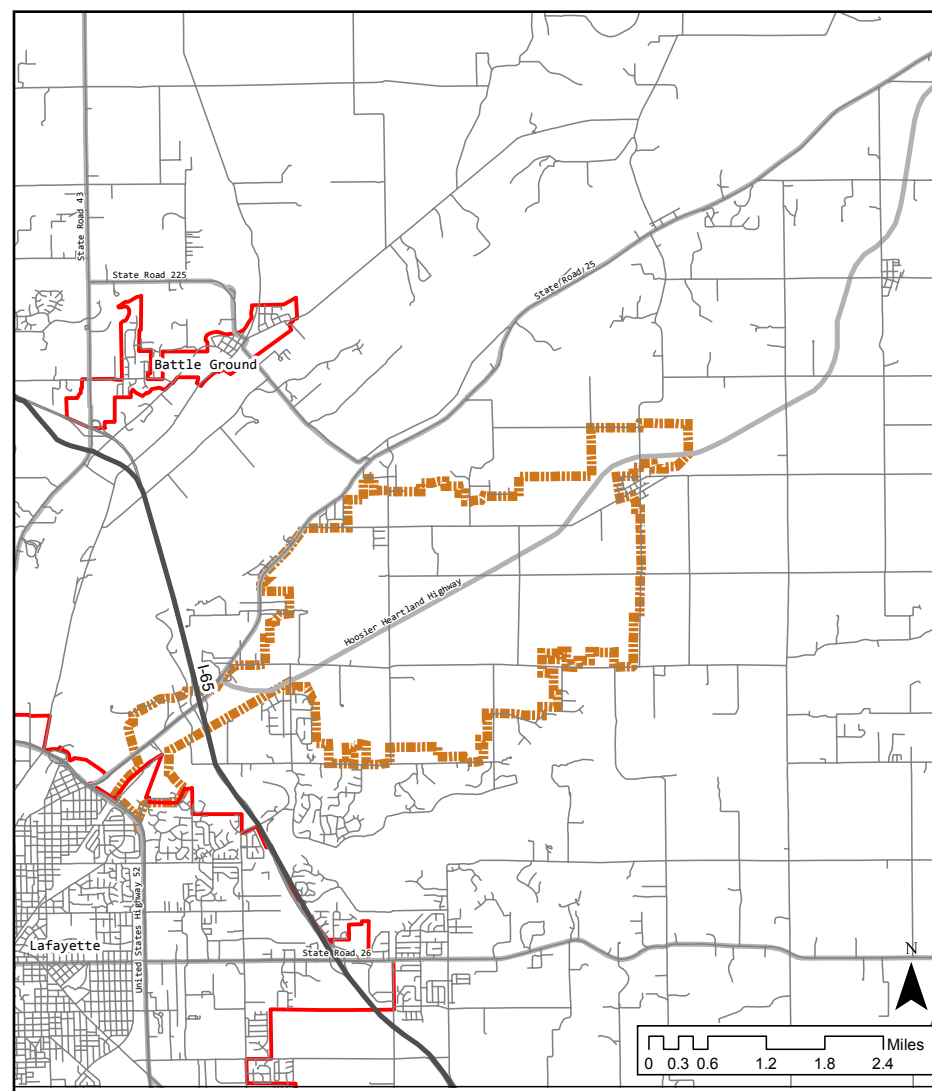
suggests long-term land uses within the study area that may or may not be consistent with current zoning. These land use recommendations are outlined to provide a tool to assist future decision making as development and redevelopment begin to occur within the study area. These suggested land uses, however, do not represent a presumptive guarantee of any future zoning approval.

Background of the Plan

This section of the Hoosier Heartland Highway Corridor is also part of the Hoosier Heartland Industrial Corridor. The Hoosier Heartland Industrial Corridor's goal was to connect the Wabash Valley (Interstate 65) to the ports of Toledo and Interstate 75 to increase economic development opportunities, support local economies, improve safety and meet design standards, and improve the efficiency and capacity of transportation. The Hoosier Heartland Highway Corridor extends from Lafayette, Indiana to Toledo, Ohio for approximately 200 miles. A portion of the Hoosier Heartland from Lafayette to Delphi is located in the study area. This portion was opened to traffic in October 2012 with the section from Delphi to Logansport opening October 2013. The Hoosier Heartland Highway was relocated parallel to the Norfolk Southern Railroad and upgraded from a two-lane facility to a four-lane, limited access highway to serve heavier traffic and connect the Hoosier Heartland Industrial Corridor. To the south and primarily following the highway between Lafayette and Logansport is the active Norfolk Southern Railroad. Within the study area, access along the highway includes four full access interchanges and two limited access interchanges. Full access interchanges along the Heartland are located at I-65, 500 E, 450 N, and 750 E.

In 2015 a tax increment financing (TIF) district, the Tippecanoe County Heartland Economic Development Area, was created in the area surrounding the Heartland Corridor, northeast of Lafayette including a small portion of the City of Lafayette and the majority in Tippecanoe County and is within parts of Fairfield, Perry, and Washington townships. The TIF district comprises about 6,300 acres (calculated using GIS file boundary) primarily used for agricultural purposes, currently. In order for the district to generate TIF funds to be strategically reinvested in the area, improvements to the land resulting in an increase in tax revenue derived from assessed value must be made.

Context Map



Legend

— Roads
— Highways

— Interstate
 Incorporated Areas
 Study Boundary

Policy Summary: *This plan is intended to guide phased development responsibly and efficiently to spark growth and support economic development for the county, the region and the state, in concert with the vision of the community and property owners and neighbors within the study area.*

This plan is intended to be a component of the future economic vitality of the study area, as well as a catalyst for improving other areas of Tippecanoe County. This area's location to highway and rail access and amount of undeveloped land gives this area an advantage and is marketable for large scale development over time and with the appropriate utilities and community services.

In an effort to capitalize on economic development assets within the area, as well as position the area to compete for economic development opportunities as they arise, the decision was made by the County Commissioners to initiate a strategic planning effort to plan for this area in Tippecanoe County. This plan summarized that effort.

Purpose and Use of the Plan

This plan is the Area Plan Commission of Tippecanoe County's (APC) guide for physical improvement and development in the study area. This is a key area for the county since it has the potential to function as a regional hub for jobs and new residents, and will serve future users of the area and commuters using the highway. With this plan, the APC, County Commissioners, City of Lafayette and other stakeholders will work toward its strategic vision, while being equipped to respond to development and growth pressures in the area. It is intended to create an area unique to Tippecanoe County, avoid repetition of development, complement existing uses in the county, and encourage additional uses that are not or have not been suitable elsewhere in the county and regionally.

This plan is not the first planning effort focused on community and economic development. Many other planning documents were considered and used to inform this plan's recommendations. These include Service Area 21, 2040 Long Range Transportation Plan Update, and the County Comprehensive Plan. The expectation is that this plan will ultimately be supported by the Area Plan Commission and adopted by the County Commissioners and City of Lafayette

as part of the Comprehensive Plan.

This plan is both comprehensive in terms of vision and goals, as well as specific in terms of principles and strategies. While this document itself does not change zoning, it provides a basis for updating existing county zoning. The county has already taken strides to achieve a community vision by establishing the TIF district. Finally this plan serves as a key guiding document to clearly and consistently express the desires of the community for this portion of the county as well as ensure that any growth and development is done effectively, efficiently, and have a positive impact on the community. This will be a critical tool to evaluate opportunities for Tippecanoe County.

Policy Summary:

This study area is a key area for the county since it has the potential to function as a regional hub for jobs and new residents, and will serve future users of the area and commuters using the highway. With this plan, the APC, County Commissioners, City of Lafayette and other stakeholders will work toward its strategic vision, while being equipped to respond to development and growth pressures in the area. This study area is intended to create an area unique to Tippecanoe County, avoid repetition of development, complement existing uses in the county, and encourage additional uses that are not or have not been suitable elsewhere in the county and regionally.

While this document itself does not change zoning, it provides a basis for updating existing county zoning. The county has already taken strides to achieve a community vision by establishing the TIF district. Finally this plan serves as a key guiding document to clearly and consistently express the desires of the community for this portion of the county as well as ensure that any growth and development is done effectively, efficiently, and have a positive impact on the community. This will be a critical tool to evaluate opportunities for Tippecanoe County.

Planning Process

The process for developing this plan was an integrated, open, and dynamic process. A steering committee was formed with representation from boards, commissions, organizations, and property owners in the area. The steering committee was launched in April 2015.

The steering committee acted as an advisory board to guide development of the vision, goals, plan foundation, recommendations and the study area's existing conditions. Through the vision development process, the group discussed key issues related to strengths, weaknesses, opportunities, and threats.

Seven Focus Group meetings were held in the county building on May 11-12, 2015. These meetings discussed further details of a number of different topics. Key stakeholders that were considered "experts" in each area were invited to attend. Stakeholders began to identify potential strategies as their area of expertise related to the plan. Subjects for discussion included economic development, land use/planning, utilities and transportation, community services, public officials, recreation and environment, agriculture, and property owners.

A public meeting was held June 30, 2015 to gather input from residents and other stakeholders. Attendees reacted to existing conditions maps, two draft land use scenario maps, draft principles and vision, and completed a visual preference survey rating a series of pictures based on design, materials, relevance for the area, etc.. County staff, steering committee members and meeting facilitators were also available to discuss the project with participants.

Residents and stakeholders were able to provide feedback online later in the process through a public survey posted from October 26, 2015 until November 16, 2015. The survey consisted of a series of questions rating the plans recommendation and implementation components.

The feedback and input from the public has shaped the direction of the plan. The public contributed by expressing information about existing conditions that only a resident would know. They also participated in conversations about the future vision for the area to meet the needs of current residents and future residents.



(THIS PAGE INTENTIONALLY LEFT BLANK)

EXISTING CONDITIONS

Study Area

Land Use

Zoning

Transportation and Circulation

Infrastructure Framework

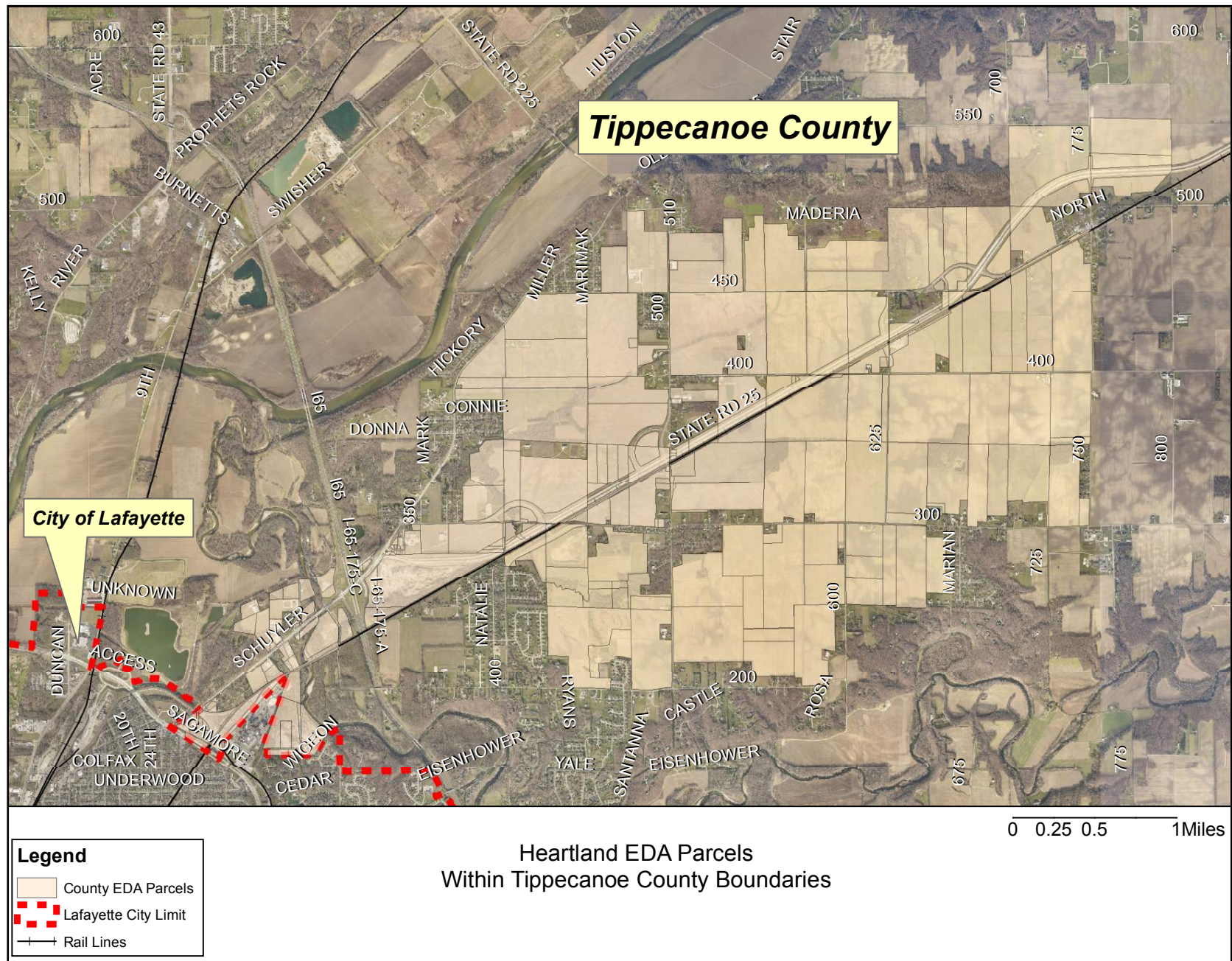
Study Area Analysis

Study Area

The study area is located along the Hoosier Heartland Highway Corridor (S.R. 25) northeast of Lafayette and southwest of Delphi in Tippecanoe County, Indiana and is within parts of Fairfield, Perry, and Washington townships. It is approximately 50 miles northwest of Indianapolis, Indiana. This section of the Hoosier Heartland Highway Corridor is also part of the Hoosier Heartland Industrial Corridor. The Hoosier Heartland Industrial Corridor's goal was to connect the Wabash Valley (Interstate 65) to the ports of Toledo and Interstate 75 to increase economic development, support local economy, improve safety and meet design standards, and improve the efficiency and capacity of transportation. Hoosier Heartland Highway Corridor extends from Lafayette, Indiana to Toledo, Ohio for approximately 200 miles. The study area is about 6300 acres and includes area within Tippecanoe County and a small portion of northeast Lafayette. The area's boundaries are roughly Old S.R. 25 and bordering natural areas to the north, 750 E to the east, 300 N to 200 N to the south, and U.S. 52 to the west, generally the area east of the intersection of State Road 25 and Interstate 65 and Buck Creek.

This area is well positioned for growth in Indiana, especially when considering new and improved access along the Hoosier Heartland Corridor, active railway through the area, and the regional location along the I-65 route between Indianapolis and Chicago. The opportunities this location presents are vast and strategically positioned in Tippecanoe County. The study area includes, in order of predominant use, a mix of agricultural, residential, natural areas, commercial, institutional, light industry, and right-of-way/interstate drainage areas.

The map (located on the previous page shows the TIF boundary created. The study area is roughly the same area as the TIF boundary without clusters of residential. Residential areas are not included within the TIF district because tax increment cannot be collected on single family residential land uses. If an area currently excluded were anticipated to redevelop, the TIF boundary may be amended to collect future increment.



Land Use

The study area includes various residential, commercial, open space, and agricultural uses. Acreages are not defined by parcel, zoning, or ownership, but by actual usage of land. These variations are:

Residential

There are about 250 single family residential units located throughout the study area in small, rural suburban neighborhoods and along County Roads 300 N, 400 N, 450 N, and 500 E. The single family homes vary in size and are generally on larger lots.

Institutional

Two churches exist in the study boundary, one in the western portion and one along 300 N.

Commercial and Industrial

Existing commercial and industrial uses are primarily located around the I-65 interchange with the exception of Crop Production Services along 625 E and Limagrain Cereal Seeds on 450 N. Many of these businesses are agribusiness focused. Other uses include a dog grooming facility, animal hospital, tire store, and gas station.

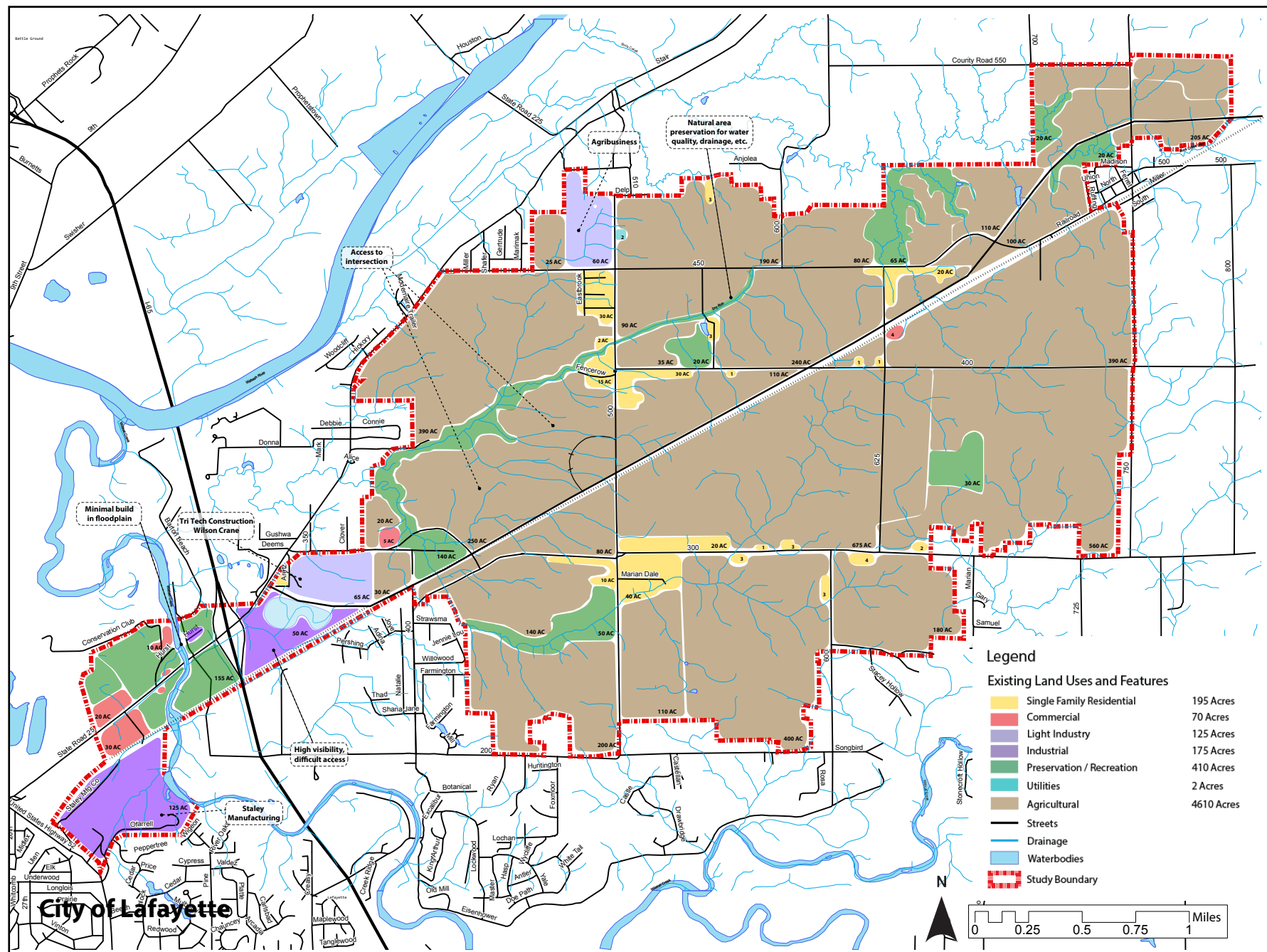
Natural Areas

Natural areas consist of wooded lots, and hydrography features including waterbodies, waterways, floodplain, and wetlands. These areas require special considerations when development occurs within or adjacent to them. Natural areas can be assets to adjacent uses providing drainage, scenic value, and open space for connectivity and recreation.

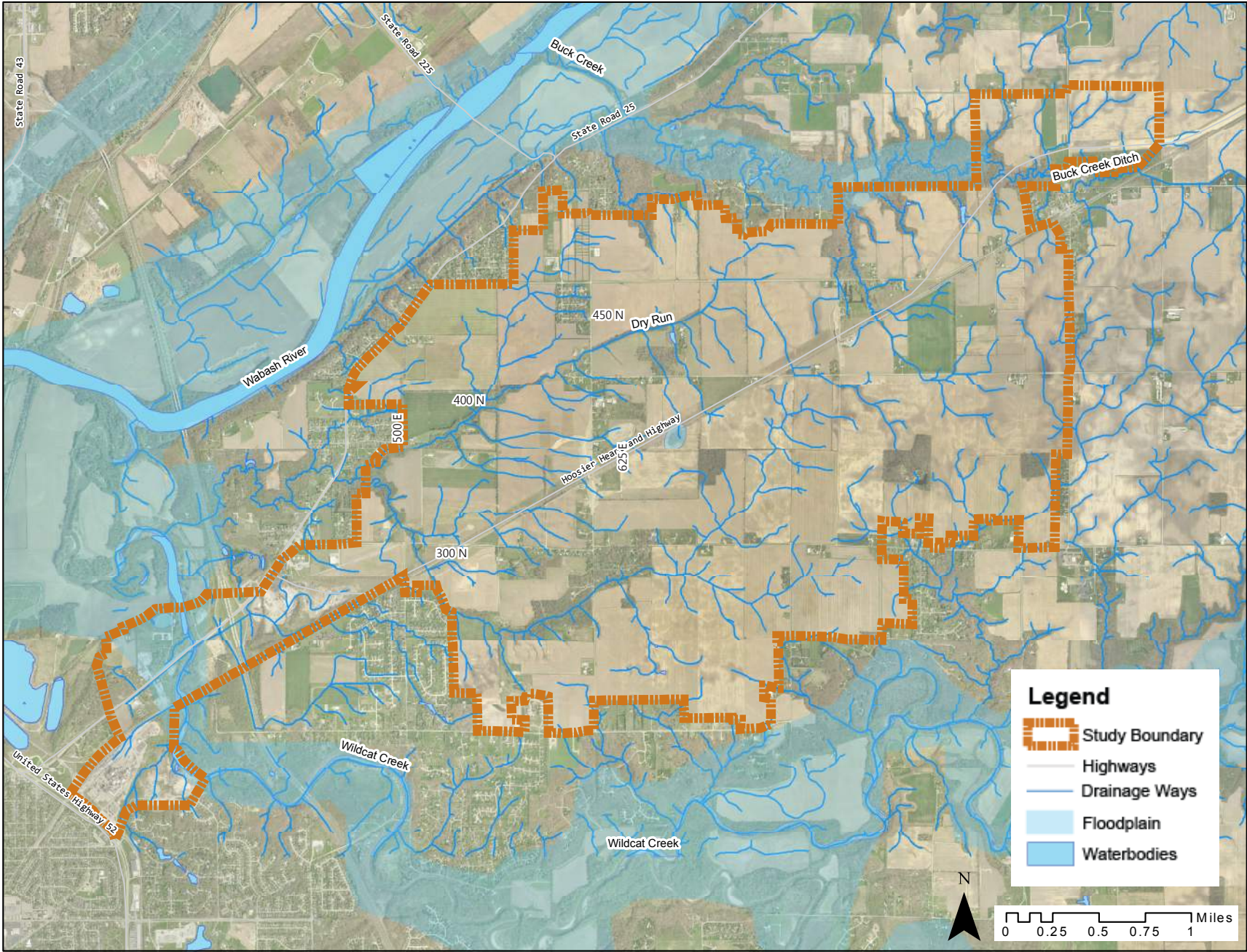
Agriculture

The study boundary consists primarily of farmland used for corn and soybean rotation. 500 E acts as a divider between types of soils dictating which land is better suited to remain agriculture. Soils west of 500 E are sandier, making them more suited for development while soils east of the road are better to remain agriculture.

Existing Land Use Map



Existing Hydrography Map



Zoning

This study area's zoning districts/classifications includes a mix of General Business, Single Family Residential, Industrial, Flood Plain, and Agriculture. Additional adjacent uses include small areas of Two-family residential, Rural Estate, and Planned Residential Development. Currently the study area consists mainly of Agriculture zoning. The Hoosier Heartland Corridor, from Interstate 65 north to the Tippecanoe County line, is also regulated by a Billboard Restrictions Ordinance that restricts outdoor advertising signs within 2000 feet of the right of way (adopted October 2015). This plan does not change the zoning. Any zoning change would have to follow state and local statutes.

Transportation and Circulation Framework

The study area's transportation and circulation system consists of local, arterial, and collector streets, a highway, an interstate, and a railroad.

Roads

Given the undeveloped nature of the study area and the new Heartland Corridor, movement east/west and north/south through the study area can be difficult given the limited access nature of the local highway system. The Heartland Corridor moves southwest to northeast, beginning in the study area northeast of the I-65 interchange extending to Toledo, Ohio. The stretch of the highway in our study area ends at 800 E.

The main east/ west connections through the study area include:

- 450 N near the northern border of the study area, connecting Old State Road 25 and Buck Creek
- 300 N south of the Heartland connecting 400 E to the edge and east beyond the study area
- 200 N acts as a boundary for some areas of the study boundary

The main north/south connections through the study area include:

- 500 E serves as a main thoroughfare with full access to the Heartland Corridor, extending north beyond the study area and ends at the south end of the study area

- 625 E is towards the eastern portion of the study area, the northern connection being 450 N and the southern connection being 300 N
- 750 E acts as a boundary for a portion of the east side of the boundary and has full access with the Heartland north of Buck Creek

Railroad

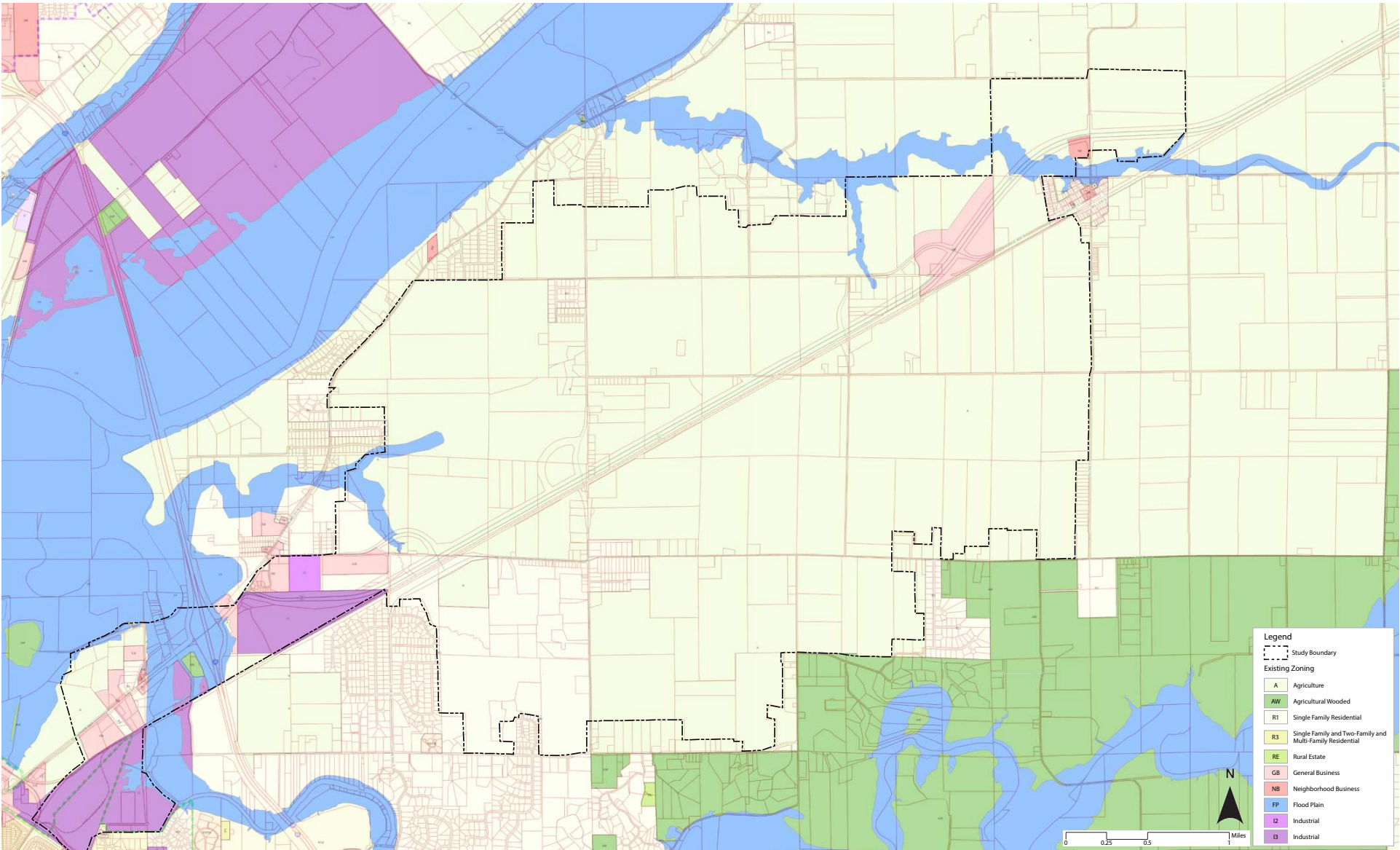
Norfolk Southern railroad transects the study area with medium to heavy traffic, generally following the Hoosier Heartland Highway Corridor. Forty trains travel this route every day from Lafayette to Fort Wayne and vice versa. There is currently minimal local service provided by rail. The railway offers opportunity for a siding serving a large industrial user.

Pedestrian and Bicycle Circulation

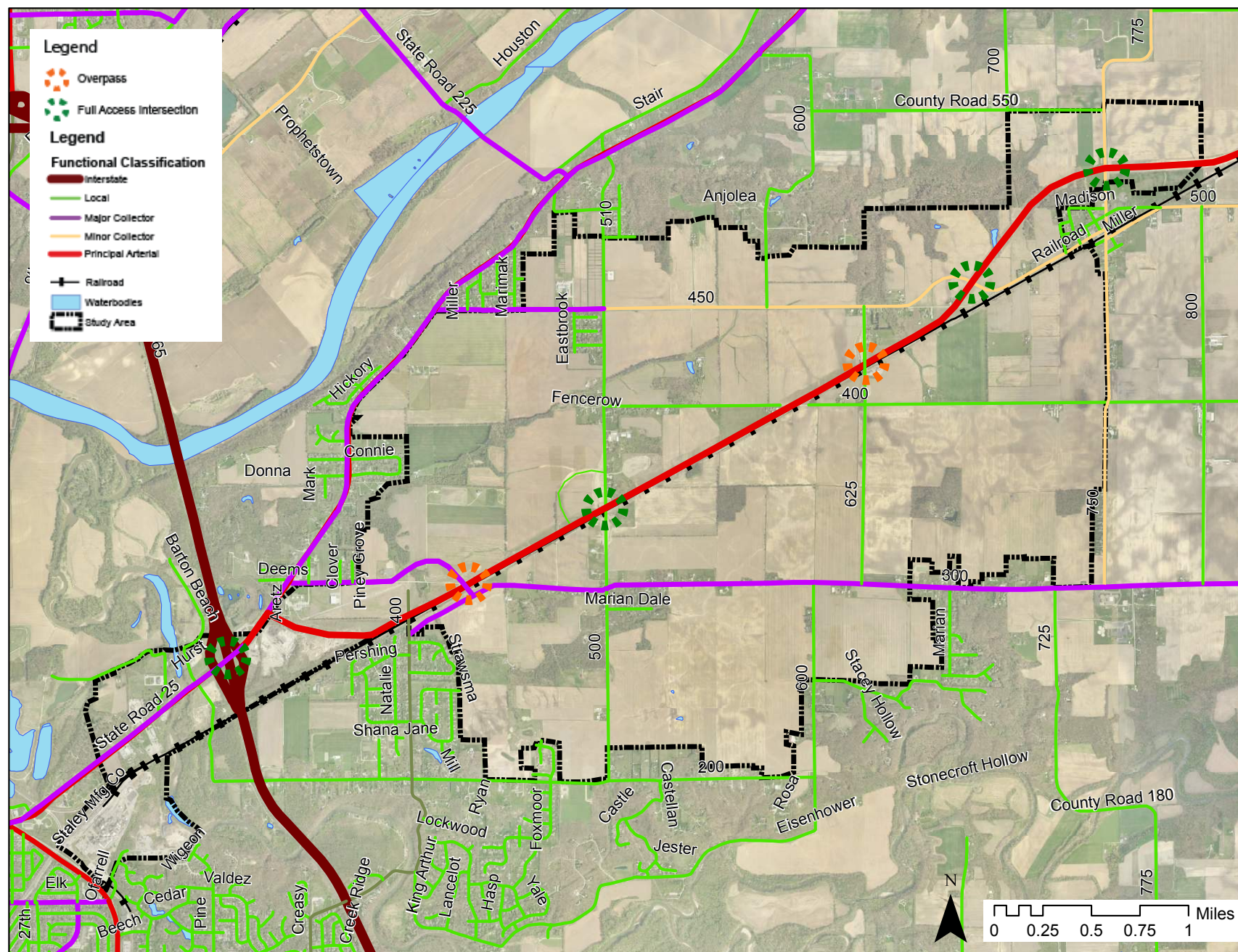
There is currently no separated pedestrian or bicycle infrastructure within the study area. Existing signed "Share the Road" routes in this area exist along 300N, a portion of 450N, 500E, and 750E. Many people walk and bike on the road or on a narrow road shoulder.

Trails in West Lafayette, Lafayette, and other parts of Tippecanoe County total over twenty miles. The Wabash Heritage trail especially has an extensive system throughout Lafayette and in Prophetstown State Park.

Existing Zoning Map



Existing Circulation Network Map



Infrastructure Framework

The study area's current infrastructure is limited but aligned with current development within the area or policies in place at the time of development.

Stormwater

The stormwater system in the study area is comprised of ditches that carry the stormwater along a roadway or property to the nearest natural body of water. With additional development, stormwater management methods will need to include municipal drains, catch basins, curbs, gutters, ditches, man-made channels, or storm drains to keep increased amounts of untreated water from entering the water supply. Fee structures should be developed to be commensurate with constructed impervious surfaces. Detention ponds were added in the area to accommodate stormwater and protect against flooding after construction of the Hoosier Heartland Corridor. Within the study area, a detention pond exists just northeast of the S.R. 25 and 500 E interchange. Small ponds also exist in subdivisions in the study area. Drainage ways throughout the area including Dry Run, Buck Creek, and many other segments are maintained to control stormwater runoff and protect land for development as well as water resources. Many of the designated drainage ways are located within a natural area/natural corridor that are encouraged to maintain or expand to control stormwater and protect agricultural land and water quality.

Sanitary Sewer

Sanitary Sewer infrastructure is provided west of I-65 by the City of Lafayette Public Works/Lafayette Wastewater Treatment. Infrastructure from Lafayette is limited to the area west of the interstate. The City of Lafayette has studied extending the utility to serve the area east of Interstate 65 for the Service Area 21 Plan completed in 2012. Current residences in the study area operate using septic systems (as was allowed by zoning at time of development). Any new development must be supported by sanitary sewer infrastructure per current regulations.

Water

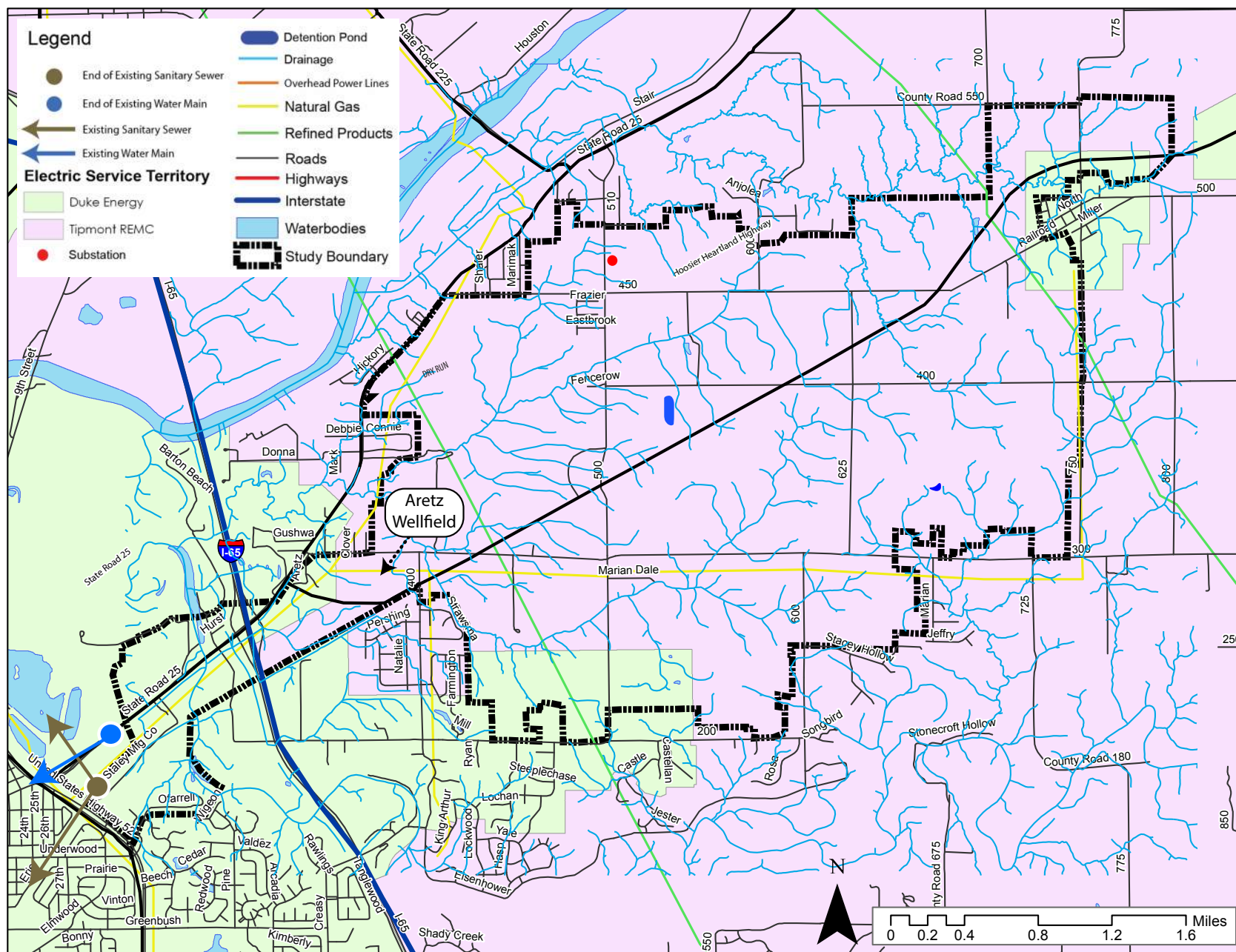
Lafayette and the county collect water from the Teays aquifer. The City Water Works Department pumps and treats water from the aquifer, and maintains four water towers. Many residents in the study area, and immediately outside, are not connected to the city's supply and receive water from wells. These residents are responsible for monitoring the quality of their own water supply. Water access from Lafayette is limited because of infrastructure does not extend east of I-65. Any development must be supported by water infrastructure, service from Lafayette. A wellfield has been proposed and development has began on a portion of the old Aretz Airport site. These wells are being created to serve both city and county users and will be readily accessible to new users in the study area.

Costs for construction were completed in 2012 for the Service Area 21 plan. The City of Lafayette produces annual reports, a water quality analysis document, and a consumer confidence report for residents viewing and is located on the City's website.

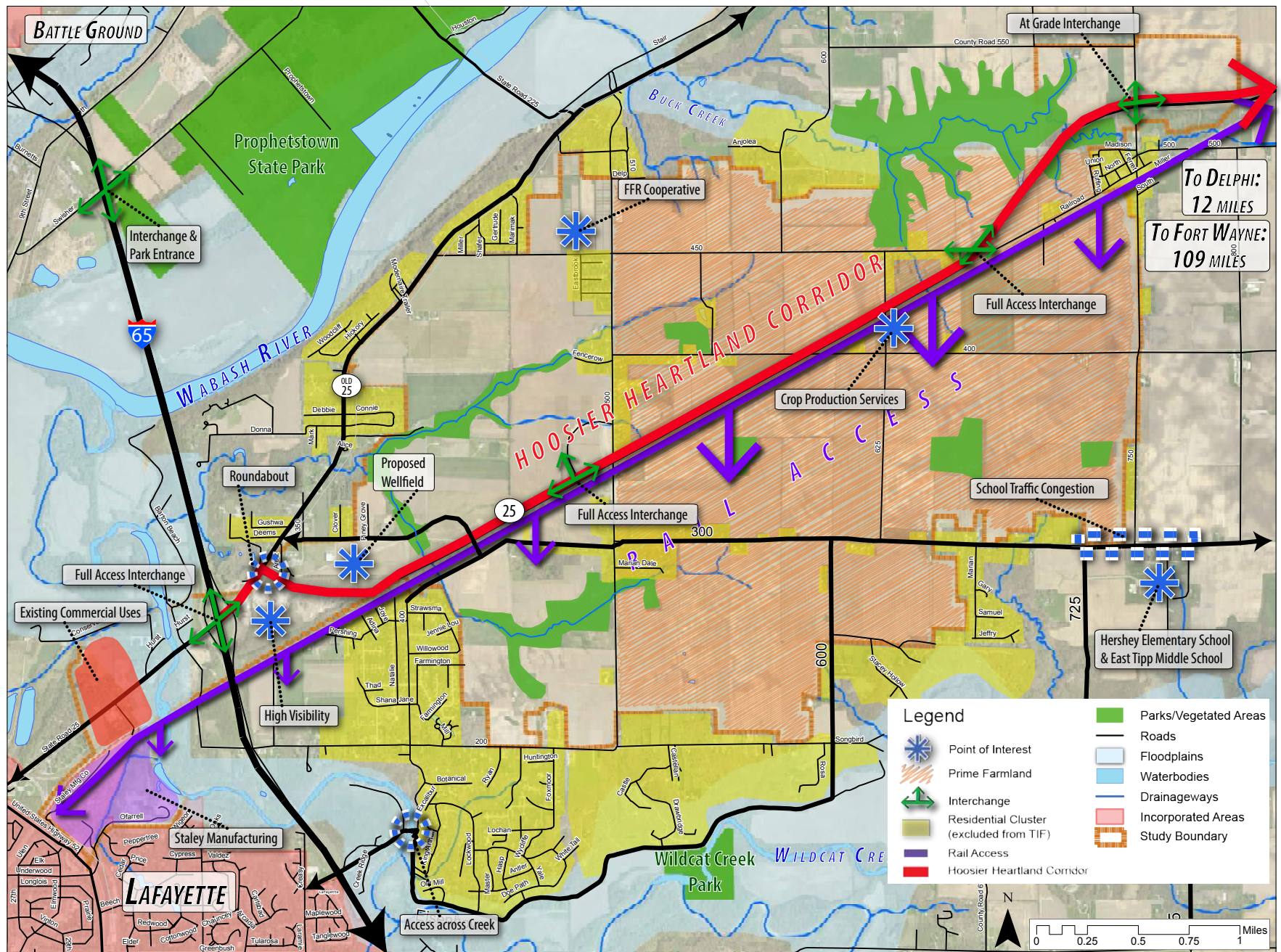
Electric

The study area has two different providers for electric. A small area of the southwest and southern part of the study area is supported by Duke Energy and the rest by Tipmont REMC. Overhead power lines are located along many county roads supporting residences and small commercial uses. There is a small substation within the study boundary in the northeast quadrant of the intersection of 500E and 450N.

Existing Infrastructure Map



Study Area Analysis Map



STRATEGIC PLAN FOUNDATION

*Vision and Goals
Strategic Plan Principles*

Vision

This vision is an overarching summary of what the community is striving for along the Hoosier Heartland Corridor in the future and is based on improvements to the area while still preserving rural character and current identity. It serves as a guide for community action and decisions and is the foundation for all recommendations of this plan.

The Heartland Corridor Economic Development Area is a unique part of Tippecanoe County as it has value in scenic and agricultural uses in its rural character but also tremendous opportunity for community supported development. The Hoosier Heartland Corridor offers new opportunities to improve economic vitality and improve quality of life for residents and businesses. Tippecanoe County will support and encourage quality, phased and balanced growth, in future areas of development and redevelopment while also promoting preservation and environmental responsibility. Tippecanoe County strives for a diverse employment base through a mix of industrial, commercial, and office developments while encouraging a variety of housing products and a sense of place that attracts residents and employees to the Greater Lafayette area to capitalize on the region's quality of life and unique natural features.

Goals

The goals set the tone for community decisions and actions that will help the community achieve the vision. They focus and direct the specific strategies and action items that must be accomplished to achieve implementation of the plan's vision. The following goals are not listed by priority; this is not a ranked list. The following should be considered policy statements.

- **Encourage balanced, phased development with public sanitary sewer and water**
Promote balanced development in phases while being mindful of preservation and conservation efforts and financial responsibility.
- **Protect natural features and promote environmental responsibility**
Maintain areas for agriculture and for conservation of greenspace to protect wildlife, resources, rural character, etc.
- **Support quality residential development**
Encourage high quality design and building standards for new residential development, with a variety of densities.
- **Enhance multi-modal circulation infrastructure systems**
Promote construction and use of multi-use trails for pedestrians and bicyclists for recreation and improved non-vehicular transportation.
- **Create quality places to live, work, and play**
Focus on developing amenities and places that serve all aspects of life.
- **Utilize a variety of economic development tools**
Explore and implement economic tools to foster local and regional growth.

Strategic Plan Principles

The strategic plan principles reflect topic areas and policy statements that have been part of exchanges with the committee, focus groups, stakeholders, and the public. These are important areas integral to successful economic development and strategic planning and specifically the implementation of this plan. Many of these issues and areas of discussion are not unique to Tippecanoe County, but there are nuances within each that are specific to the Heartland Corridor study area and the issues that impact it. Each plays an important role in the overall economic development fabric of the community. These principles are discussed to illustrate the position of the County and the committee regarding these particularly important issues. These principles, also referred to as policy statements, influenced the strategies and recommendations of this Plan as did the Vision and Goals.

Vision and Direction

The vision of this plan outlines the direction of Lafayette and Tippecanoe County for economic development and preservation in the study area around the Hoosier Heartland Highway Corridor. The development of this vision was the result of many committee meetings and discussions. It was important for the vision elements to be comprehensive, yet tailored to the current issues and conditions in Tippecanoe County. Focusing on the strengths and assets, yet recognizing the weaknesses and challenges, allowed the vision and goals pieces to set a bold and promising direction for the area.

The reality for all communities is that growth is a necessity if communities are to be sustainable in the long term. As costs of service increases, communities must continue to grow their tax base and population in order to maintain services, let alone expand or enhance services beyond current levels.

Beneficial growth and development in communities occurs when there are synergistic relationships among market conditions, property owners' desires, and community need. Based on the stakeholder and steering committee data gathered as part of this planning effort, any growth within the study area is desired to be in the form of high end, rural residential, and concentrated,

high quality local businesses, office, and industry with substantial buffering to preserve scenic character. It will also be important for Tippecanoe County to understand the needs of the new businesses and their prospective employment base. Tippecanoe County needs to provide this new workforce with the amenities, services, and quality of life that are demanded in today's climate.

While this plan outlines land uses for specific areas within the study area, it is imperative the plan and the leadership remain committed to the vision while being flexible. With flexibility in mind, there are many changes that can affect the ability for economic and community development to occur. As the form of that development take shape, it may also vary based upon changes in the future that are unforeseeable today. Development opportunities will be presented for this area; some will perfectly align with the vision for the study area and some will be in conflict with it.

These scenarios require the county's leadership to follow the vision and approve, ask for modifications, or deny these opportunities. Ultimately, vision is paramount, and although the plan must provide flexibility, the community's vision shall be maintained, and all decisions that face the leadership of the community shall be judged and colored by this vision.

Policy Summary: *Any growth within the study area is desired to be in the form of high end, rural residential, and concentrated, high quality local businesses, office, and industry with substantial buffering to preserve scenic character.*

Policy Summary: *In order to be competitive, Tippecanoe County needs to provide this new workforce with the amenities, services, and quality of life that are demanded in today's climate.*

Phased and Balanced Development

This plan recognizes that the recommendations made are not short-term decisions and are likely to change in the future. Growth within the study boundary is dependent on many different factors including need for utilities and other infrastructure, and marketing and incentives for new developments. Because of these factors, development is encouraged to be phased, balanced,

and mindful of natural areas. The rural character of the area is highly desired and appreciated by residents. This should be kept in mind as development of any type occurs and any higher intensity land uses should be buffered. It is expected that areas with the best access and closer alignment with infrastructure will develop sooner than those that require significant infrastructure expansion. This generally means that development is likely to occur from the I-65 interchange first and out towards Buck Creek as utilities allow.

Policy Summary: *Growth within the study boundary is dependent on many different factors including need for utilities and other infrastructure, and marketing and incentives for new developments. Because of these factors, development is encouraged to be phased, balanced, and mindful of natural areas.*

Emphasis on Preservation

The scenic character of this area in Tippecanoe County is what makes it so unique and desirable for people to live. Many of the current residents farm the agricultural land or intentionally moved to this area for the vistas, scenery, and present character. Maintaining the rural character will be important for current residents while creating amenities to attract new, and a variety of residents. Much of the study area is farmland or natural areas, and are encouraged for preservation for character, economic, and functional reasons. There should be a conscious effort to screen and buffer higher intensity land uses to preserve scenic character. Methods of buffering can include other land uses, landscaping, mounding fencing, etc.. Concentrating development around full access interchanges on the Hoosier Heartland Highway Corridor and at the I-65 interchange will also aid in maintaining character while supporting growth and economic development. Natural areas support wildlife, drainage, water quality, air quality, scenic value, and recreation and should be preserved to encourage the continued functioning of these systems. Expanded natural areas may provide areas for passive recreation, and other outdoor activities while still promoting normal environmental function.

Policy Summary: *There should be a conscious effort to screen and buffer higher intensity land uses to preserve scenic character. Methods of buffering can include other land uses, landscaping, mounding fencing, etc.. Concentrating development around full access interchanges on the Hoosier Heartland Highway Corridor and*

at the I-65 interchange will also aid in maintaining character while supporting growth and economic development. Natural areas support wildlife, drainage, water quality, air quality, scenic value, and recreation and should be preserved to encourage the continued functioning of these systems.

Quality Places to Live, Work, and Play

Conditions in a community will directly influence the choices people make for themselves and their families throughout their lives. These conditions play an important role in our ability to make these choices. The economic, social, and physical environments greatly impact our health and disparities in quality environments, infrastructure, and housing can pose obstacles for less fortunate residents of any area. Targeting investments that promote quality places to live, work, and play will help lead residents to have healthier, happier, more productive lives. These investments should be evaluated for both their economic impact and their contribution to quality living, working, and playing for residents and employees. Achieving this can create more stable property values, increased communication in the community, healthier lifestyles, higher profits for businesses, appreciating tax base, and more. With these three qualities of places met, quality of life overall is enriched.

Policy Summary: *Any investments should be evaluated for both their economic impact and their contribution to quality living, working, and playing for residents and employees.*

Long-term Commitment

It is anticipated that land uses within the study area will change as market conditions evolve. This plan is intended to guide development to the appropriate locations when the opportunity presents itself and serves as a guide that allows flexibility and creativity. Over the long-term, many changes will occur. Some parts of the study area are likely to present economic development opportunities before others.

Any land use definition as part of this planning effort is intended to represent a long-term view of the study area. While there may be opportunities that will present themselves in the near-term, the development of the study area

will occur over time. It is important to be cognizant of the adjacent land uses. Buffering land uses and landscaping will be critical to higher intensity development. Retail uses are intended to serve the local community, and visitors, and build from the current assets of the local area, and are discouraged if it would detract from the overall vision of the county.

Policy Summary: *This plan is intended to guide development to the appropriate locations when the opportunity presents itself and serves as a guide that allows flexibility and creativity. Any land use definition as part of this planning effort is intended to represent a long-term view of the study area. It is important to be cognizant of the adjacent land uses. Buffering land uses and landscaping will be critical to higher intensity development. Retail uses are intended to serve the local community, and visitors, and build from the current assets of the local area, and are discouraged if it would detract from the overall vision of the county.*

Impact on Other Communities

This plan focuses on the Hoosier Heartland Highway Corridor, but not without looking at the whole community and context. Other areas of industrial and commercial development remain a key focus for the county. The recommendations for this area support the county, but not at the expense of Lafayette and West Lafayette or other areas in the county. New, build-to-suit users will likely develop at the interchanges along the corridor due to its access, visibility, context, and benefits of undeveloped land.

Lafayette and West Lafayette are key areas to pay attention to because of their significant role in the county. Lafayette is just southwest of the study area, and contains many amenities the residents of the area use on a daily basis. West Lafayette has become a significant area for the county being home to Purdue University. It is important to create a proactive development plan that will ensure the long-term vitality of these cities. Ensuring this will require careful consideration of types of developments and businesses so as to not duplicate unique services offered by Lafayette and West Lafayette. For example, this would include uses similar to the Purdue Research Park and commercial uses in the northeast area. Businesses and organizations that would be unique to the region should be the targeted development for the study area. Close proximity to these cities should not create conflict and

competition but rather synergy for an overall greater and economically viable community.

Policy Summary: *It is important to create a proactive development plan that will ensure the long-term vitality of these cities. Ensuring this will require careful consideration of types of developments and businesses so as to not duplicate unique services offered by Lafayette and West Lafayette. Businesses and organizations that would be unique to the region should be the targeted development for the study area. Close proximity to these cities should not create conflict and competition but rather synergy for an overall greater and economically viable community.*

Infrastructure – Road/Rail/Pedestrian/Bicycle

This plan recognizes that any change in development will affect the transportation system. Recommendations have been made for an enhanced transportation network to include new roads, improvements and extensions of existing roads, improvements to existing intersections or new intersections, and additional rail access when it is appropriate. Future transportation improvements must be sensitive to the overall transportation network of the area and the greater Tippecanoe County community and provide facilities for multiple modes of transportation including pedestrian and bicycles. As such, future road infrastructure improvements will likely warrant specific design criteria with regard to road cross-sections, lane widths, and intersection design.

Policy Summary: *Recommendations have been made for an enhanced transportation network to include new roads, improvements and extensions of existing roads, improvements to existing intersections or new intersections, and additional rail access when it is appropriate. Future transportation improvements must be sensitive to the overall transportation network of the area and the greater Tippecanoe County community and provide facilities for multiple modes of transportation including pedestrian and bicycles. As such, future road infrastructure improvements will likely warrant specific design criteria with regard to road cross-sections, lane widths, and intersection design.*

Infrastructure – Utilities

As growth occurs, it will be important to monitor the impacts of that growth and make alterations and adjustments to utility infrastructure as necessary.

Currently, Lafayette water and sewer utilities do not extend into the study area, but stop west of the I-65 interchange. Residents in the area are being served by private wells and septic systems for water and sewage. In order for any additional development to occur, public utilities will be required to serve the study area. Preliminary cost estimates for these extensions are located in Appendix A. This plan recommends additional study to determine alternatives.

Policy Summary: *In order for any additional development to occur, public utilities will be required to serve the study area and support desired development, as required by zoning. Appropriately sizing and phasing this infrastructure will be critical to achieving the vision and goals of the study area and the communities.*

RECOMMENDATIONS

Future Land Use Plan

Land Use Descriptions

Circulation Infrastructure

Utility Infrastructure Plan

Economic Development Tools

Site and Design Standards

Future Land Use Plan

The Future Land Use Plan (see map to the right) is a long-range conceptual plan illustrating potential land uses that could support the vision, goals, and principles for economic development set forth in this document. This plan (map) is an integral tool for economic development and is the result of a series of analytical and detailed analysis and conversation throughout this planning process. Factors that informed this map include:

- Zoning
- Property owners
- Transportation and access
- Utility availability
- Natural features
- Environmental constraints
- Steering committee input
- Stakeholder input

Some of the land uses and districts identified on this map are intended to be an enhancement of current uses and are supported by appropriate zoning. Other uses and districts identified on this map suggest a change in land use. This proposed change in land use will be something that happens over time and will happen when the market, the property owners and the County approval process aligns. This plan does not change the current zoning of the districts that are not consistent with the future land uses.

Policy Summary: *This proposed change in land use will be something that happens over time and will happen when the market, the property owners and the County approval process aligns. This plan does not change the current zoning of the districts that are not consistent with the future land uses.*

Description of Land Use Components

All Districts

Each land use district is intended to identify strategic areas for long-term desired uses. Both the location and type of use was determined weighing a multitude of factors including, but not limited to, accessibility, key site characteristics,

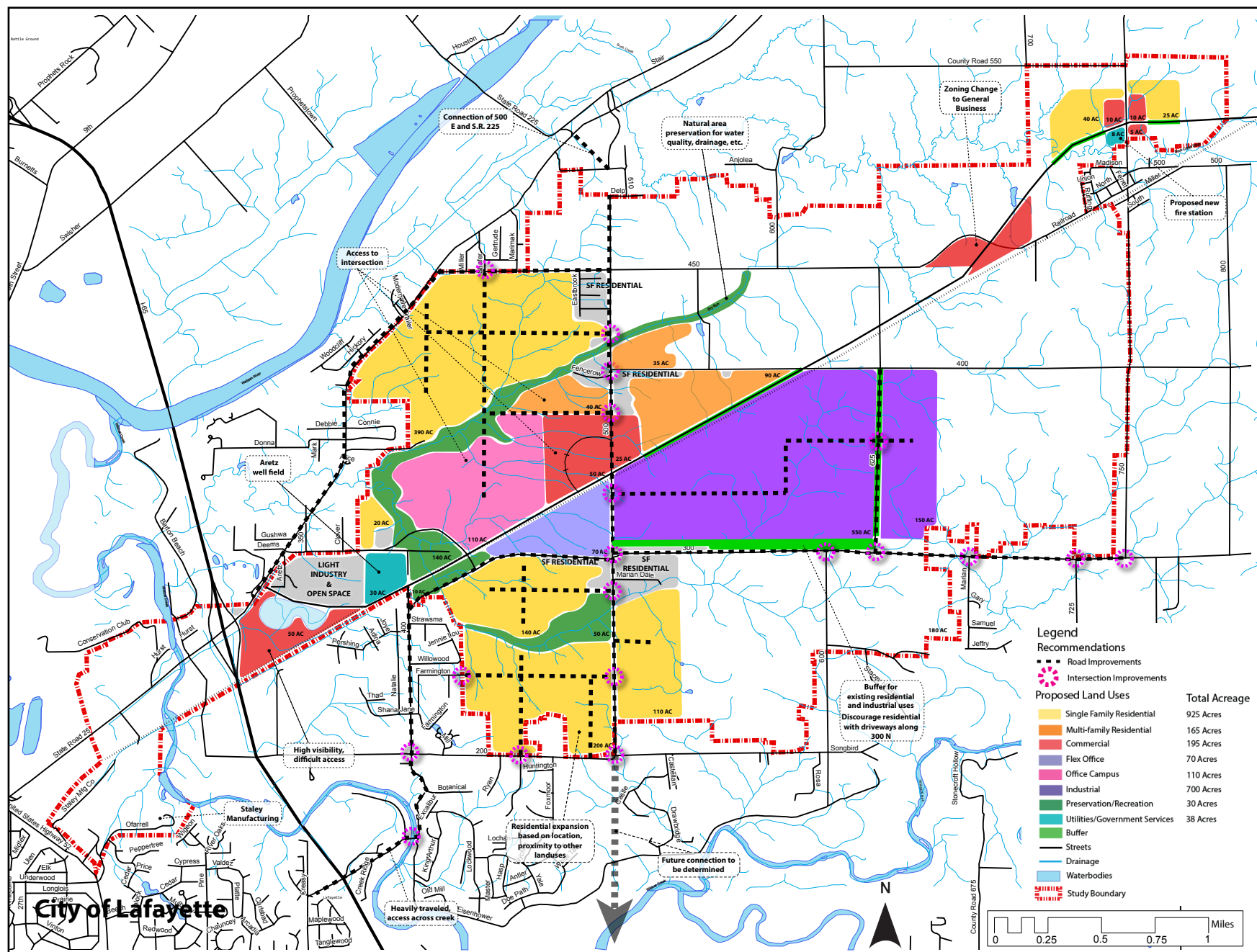
visibility, and adjacent land uses. It is recognized that all existing users are heavily invested in their property and their community. All development is market driven, and must include a willing buyer, willing seller, and necessary financing, and municipal approval and proceedings. It is encouraged that the development process for any potential new development goes through the path with the least resistance.

As development or redevelopment occurs around any existing use, great sensitivity should be given to how new uses complement the existing ones, and how the impact of the new development can be mitigated as best as possible. All districts should have an emphasis on high-quality architecture, site design, proportion, and detail. In the event that redevelopment occurs or immediate, short-term improvements could be made, specific efforts should emphasize the creation of buffers and screens to/from higher intensity land uses. This transition area should be allowed in all districts that are immediately adjacent to a non-related or non-conductive land use.

In the end, form is key when establishing a vision and a driving force for character within an area. Use is important, and should always be considered, and a sympathetic approach should be taken with the existing user when considering a proposed non-conductive adjacent use. However, when form is considered above use, then quite often adjacent uses can transition seamlessly, without significant notice. It is these seamless transitions that build the fabric and character of a community.

Policy Summary: *All development is market driven, and must include a willing buyer, willing seller, and necessary financing, and municipal approval and proceedings. As development or redevelopment occurs around any existing use, great sensitivity should be given to how new uses complement the existing ones, and how the impact of the new development can be mitigated as best as possible. In the event that redevelopment occurs or immediate, short-term improvements could be made, specific efforts should emphasize the creation of buffers and screens to/from higher intensity land uses. This transition area should be allowed in all districts that are immediately adjacent to a non-related or non-conductive land use. Use is important, and should always be considered, however, form should be considered above use.*

Proposed Land Use Map



Single Family Residential

This district focuses on single-family detached dwelling units. This area will include low density/rural single-family homes similar to existing residences within the area and may include medium-density residential for efficiency of services and infrastructure. If is preferred that residences would be master planned as neighborhoods rather than developed as single units in order to create unique identities and character desired potential residents. These uses, like the other uses in the study area, are expected to be high quality and mid-to high-end to diversify the single-family residential product in Tippecanoe County. These new neighborhoods are also expected to meet the current residential needs of the community and serve potential new employees and employers of the immediate vicinity. Proposed single-family residential uses comprise of approximately 925 acres.



Multi-Family Residential

This district focuses on multiple-family attached dwelling units with a density of 5 to 22 units per acre. These areas may include townhomes, row houses, duplexes, quadplexes, and apartments depending on the market demand and desired resident types. These uses, like the other uses in the study area, are expected to be high quality and market rate to diversify the multi-family residential product in Tippecanoe County and to meet the current residential needs of the community and capitalize on potential employees relocating to the area in response to new office and industrial employers. Higher-quality finishes and amenities will be key to attracting young professionals. Multi-family complexes and developments should contain quality of life amenities including wireless connectivity, outdoor spaces, walkability, and close proximity to recreation, shopping, and dining options. Multi-family residential uses comprise of approximately 165 acres.



Commercial

This district focuses on the broad use of commercial, including general/highway commercial goods and services establishments, general retail, neighborhood commercial and local businesses, and hotel. The primary market of these uses includes the community as a whole, nearby communities, adjacent neighborhoods and commuters passing through the area. Given the access, visibility, and potential future traffic volumes associated with the Heartland Corridor and the proximity to I-65, commercial uses are expected to develop here. Commercial uses primarily should focus on more vehicular intensive uses due to the location within the community and should complement rather than compete with other significant commercial corridors in the region and the downtown area of Lafayette.

Priority should be given to local businesses and neighborhood commercial given their economic benefits versus chain retail establishments, but should not hinder a desired business from developing in the study area. Destination retail may also be an opportunity given the traffic counts along I-65. Population density in the district will be key to determining the types and mix of dining options that are warranted. Density will also provide the critical mass needed to support retail opportunities, such as small strip retail or potential outlet shopping destinations.

Accessibility to major thoroughfares will be critical and commercial uses should be focused near the interstate (within ½ mile) and at interchanges along the Heartland Corridor. Because this is a significant gateway for the region and commercial uses will develop in highly visible areas, high quality architecture and overall site design will be critical and will set the tone for the entire area. Proposed commercial uses comprise of approximately 195 acres.



Office Campus

This district allows a variety of office users including, but not limited to, professional office, medical office research facilities with lab space, technology companies, and other primarily office employers. The district would include multi-story (up to four stories) Class A and B office buildings in a lower density campus like setting. Users in this district would generate moderate traffic. The different uses within the campus district should support and complement the others. Office Campus districts should have visibility from the highway but can be farther from access locations because it is a destination use and does not need direct access. Minimal, lower intensity commercial uses may be scattered throughout to support employees within the campus. This district should emphasize attention to detail, site design, and high quality architecture by utilizing consistent design standards. Office Campus uses comprise of approximately 110 acres.



Flex Office

This district focuses on a mix of professional office and flex office uses such as office parks, employment centers, incubator businesses, research facilities, and more. This district will be low density, one to two story buildings and generate little to moderate traffic. This use will have some visibility from the highway but direct access is not necessary. Office buildings will be flexible spaces for a variety of uses including small offices, warehousing, space for light assembly, and truck bays if necessary. Flex office spaces should support and complement both the office and industrial districts nearby. Because of the visibility from the highway, high quality architectural and site design standards should be encouraged. Flex Tech/Office uses comprise of approximately 70 acres.



Industrial

This district focuses on a mix of uses (including light/advanced manufacturing, assembly, service, agribusiness distribution, warehouse, and wholesale establishments) that are clean, quiet, enclosed and free of hazardous or objectionable elements. With rail access to the south of S.R. 25, interchange access to the highway, and large developable fields, with owners permitting, a large industrial user or collection of users would be likely and most beneficial to the area. This district will generate moderate traffic including trucks and rail. A potential rail spur will extend south from the rail corridor between roads 500N and 625N. Rail access is a potentially significant asset to this industrial district and differentiator for the industrial market. Users such as power plants, milling, mining, refineries, etc. should be discouraged. Given the higher impact of all industrial uses, effective screening and buffering is essential in this district. Proposed industrial uses comprise of approximately 700 acres.



Preservation / Recreation

This district focuses on preserving natural areas not suitable for development (for reasons of being a cemetery, floodplain/floodway, nature preserve, wetland, water body, significant stands of trees, and/or steep terrain) with owner's approval and participation in the process as well as open space for passive recreation. Much of the area recommended for conservation/open space is currently wetland, floodplain, wooded area, drainage way, or water body. There should be an effort to use the conservation areas and areas within other districts to provide a continuous linear greenspace for users of the buildings in the districts, as well as community wide and to connect to the larger open space network through the utilization of paths and connection of spaces. Passive recreation examples include undeveloped space for gardens, hiking trails, nature observation, etc. Proposed preservation and recreation uses comprise of approximately 30 acres.



Utilities/Government Services

Growth and development will require expanding utilities and government services in the study area. This land use will include area for utility expansion for electric, water, and sanitary sewer like the City owned Aretz wellfield being developed to service water to the area. Government services are also included in this land use to provide meeting space for government bodies, police and fire departments, other emergency services, public library, and public meeting space. Utility and government service uses comprise of approximately 38 acres.

Agricultural

Large portions of the study area are currently designated as agricultural. These existing uses create unique experiences and destinations for Tippecanoe County. It is intended and encouraged that these uses continue to be embraced as an amenity within the community. These established agricultural uses are key to attracting future businesses and creating an agriculturally driven experience for future development. Expanding on the existing resources in the area, future development potential includes agritourism and creating a destination for the regional area. Agricultural uses comprise of approximately of 2,325 acres.



Circulation Infrastructure

Pedestrian and Bicycle Infrastructure

Future development within the study area should ensure that pedestrian and bicycle circulation is accommodated. The type of pedestrian and bicycle facility will vary but could include in many different combinations, sidewalks, bicycle lanes, separated multi-use paths, and shared lanes. The physical buildings and infrastructure are encouraged to use principles that support pedestrian connectivity and pedestrian scale, as well as proper pedestrian and urban proportions. A key design principal is to pay as much attention to the pedestrian and the cyclist as is paid to the car. So often these alternate modes of transportation are an afterthought, only considered after the vehicle's needs are met. A balanced approach to transportation adds to the quality of life and place that is created. Streetscapes that allow for balanced transportation systems would be an appropriate improvement near the S.R. 25 and 500E intersection where higher density development is likely to occur. Streetscapes should reflect quality and detail. In general, the streetscape should consist of unified elements including street lights, street trees, perennial plantings, sodded parkways, continuous sidewalks, pedestrian crossings, pavement markings, and bicycle facilities where appropriate.



Vehicular Infrastructure

Increased development in the study area will result in necessary road improvements to accommodate increased traffic and users. Infrastructure improvements will need to include modifications to existing roads and intersections and construction of new roads to access future land uses and accommodate new developments. Improvements are especially important for roads near full access interchanges or intersections at I-65, 500 E, 450 N, and 750 E where future development is encouraged to occur. Other roads that are currently insufficient for existing traffic include Eisenhower Road where the alignment and lane widths are troublesome for existing traffic, a second connection across the Wildcat Creek from SR 26 to access future industrial development proposed in this plan, and 300 N around Hershey Elementary School and East Tipp Middle School to ease congestion and accommodate additional future residents.

Rail Infrastructure

The location of the Norfolk Southern rail corridor and the types of service it provides to the region is a great opportunity to utilize a siding to create a rail-served industrial site. Providing rail access to the land south of the Hoosier Heartland Corridor will create a product that is rare in the market and will cater to a large industrial user. The addition of the rail siding provides for a significant industrial market, and is an amenity that is highly desirable and unique in today's industrial market given the available contiguous land and access to multiple highways in the region. By creating rail sidings and rail served industrial properties, Tippecanoe County can set itself apart from other communities and

offer attractive sites for advanced manufacturing or distribution facilities that can take full advantage of the rail and interstate transportation systems that are available. Upon completion of the siding and other utilities, the county should look to obtain a Norfolk Southern select site designation to further distinguish the industrial opportunities in this area.

Policy Summary: *All circulation improvements, pedestrian, bicycle, vehicular and rail, should be planned to meet the long-term goals of the land use plan and all safety and functional requirements. Circulation infrastructure, roads, intersections, bridges, sidewalks, bicycle facilities, and trails are an important strategy to facilitate economic development within the study area. Projects within the study area should be evaluated as to the benefit to the whole community and the overall transportation network. It is recommended that when improvements are made through a phased approach, that all right of way be acquired at the beginning to streamline the improvement and upgrade of the facility in the future.*

Policy Summary: *It is understood that any development that occurs within the study area will bring an increased amount of traffic which current conditions cannot support and are not currently planned for in the current transportation plans. Some roads will need to be upgraded in order to support additional traffic. The need for upgrades will be based on proposed future land uses and will accommodate alternative modes of transportation including pedestrians and bicyclists.*



Utility Infrastructure

Investments in public infrastructure, such as transportation and utility systems, are an important strategy to facilitate economic development within the study area. Potential infrastructure improvements include transportation upgrades such as roads and intersections and a rail siding. Utility extensions, such as water mains and towers and sanitary sewer systems, are also key strategies to foster economic development. Infrastructure development strategies may also include storm sewer improvements and regional detention facilities. All infrastructure improvements, if paid for locally, will defray development costs associated with private developments and make the study area more attractive for private investment. Therefore, the county should be proactive in implementing these utilities to strategic properties as an incentive for developers to locate within the study area.

Infrastructure investments are a proven tactic necessary for the development of the study area to facilitate and encourage private development and investment. Improvements in infrastructure include water mains, sanitary sewer, stormwater and regional detention, and road and intersections.

Policy Summary: All utility improvements, regional detention, green infrastructure and stormwater, drinking water, sanitary sewer, should be planned to meet the long-term goals of the land use plan and all safety and functional requirements. Utility infrastructure is an important strategy to facilitate economic development within the study area. Projects within the study area should be evaluated as to the benefit to the whole community and to each of the utility systems. It is recommended that when improvements are made through a phased approach, that all right of way be acquired at the beginning to streamline the improvement and upgrade of the facility in the future. It is understood that developments beyond the currently allowed by zoning, small-scale residential developments, would require municipal utilities and services. Additional studies are required to fully determine the costs and strategy for utility infrastructure in the study area.

Regional Detention

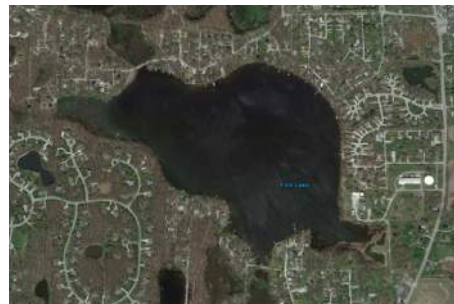
As development and growth occur, regional detention ponds or facilities will need to be created to accommodate stormwater runoff. Both small scale residential and large scale commercial and industrial ponds and facilities will be necessary to prevent flooding and capture the untreated water from driveways, gutters, streets, parking lots, etc. Regional detention creates efficiency by not requiring each individual user to meet detention requirements on their property.



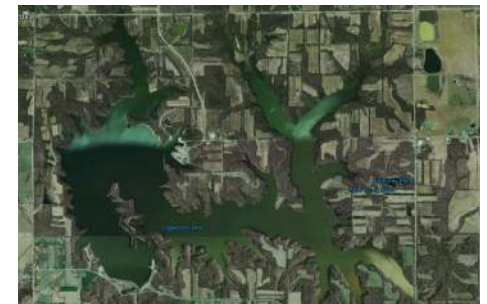
Small scale stormwater pond (<1 Acre)



Mid scale regional detention (8 Acres +/-)



Large scale regional detention (100 Acres +/-)



Extra Large scale regional detention (1000 Acres +/-)

Green Infrastructure

Implementing green infrastructure will help to mitigate the impacts of heavy stormwater on land. Flooding is reduced by slowing and reducing stormwater discharges while decreasing the amount of water that would drain directly into the sewer or the Wabash River and Wildcat Creek. Water quality is increased by green infrastructure treating stormwater and preventing untreated flows from entering the area's waterbodies. Last, green infrastructure increases efficiency of the water supply system and decreases capital costs of grey infrastructure. The county should incentivize new and innovative development practices that include natural stormwater treatment systems in order to reduce the impact any new development has on the environment. Such practices include the following:

- Rain gardens
- Bioswales
- Bioretention
- Wetland mitigation and banking
- Graywater harvesting and reuse
- Regional Detention

The Tippecanoe County Partnership for Water Quality is an organization that provides education and volunteer opportunities to help increase the quality of local water resources and improve watersheds. Utilizing this program in the area will help to educate students, residents, businesses, etc. about using green infrastructure projects and other best management practices to manage stormwater runoff.

Rain Garden



Bioswale



Bioretention



Wetland Banking



Greywater Harvesting and Reuse



Regional Detention



Water

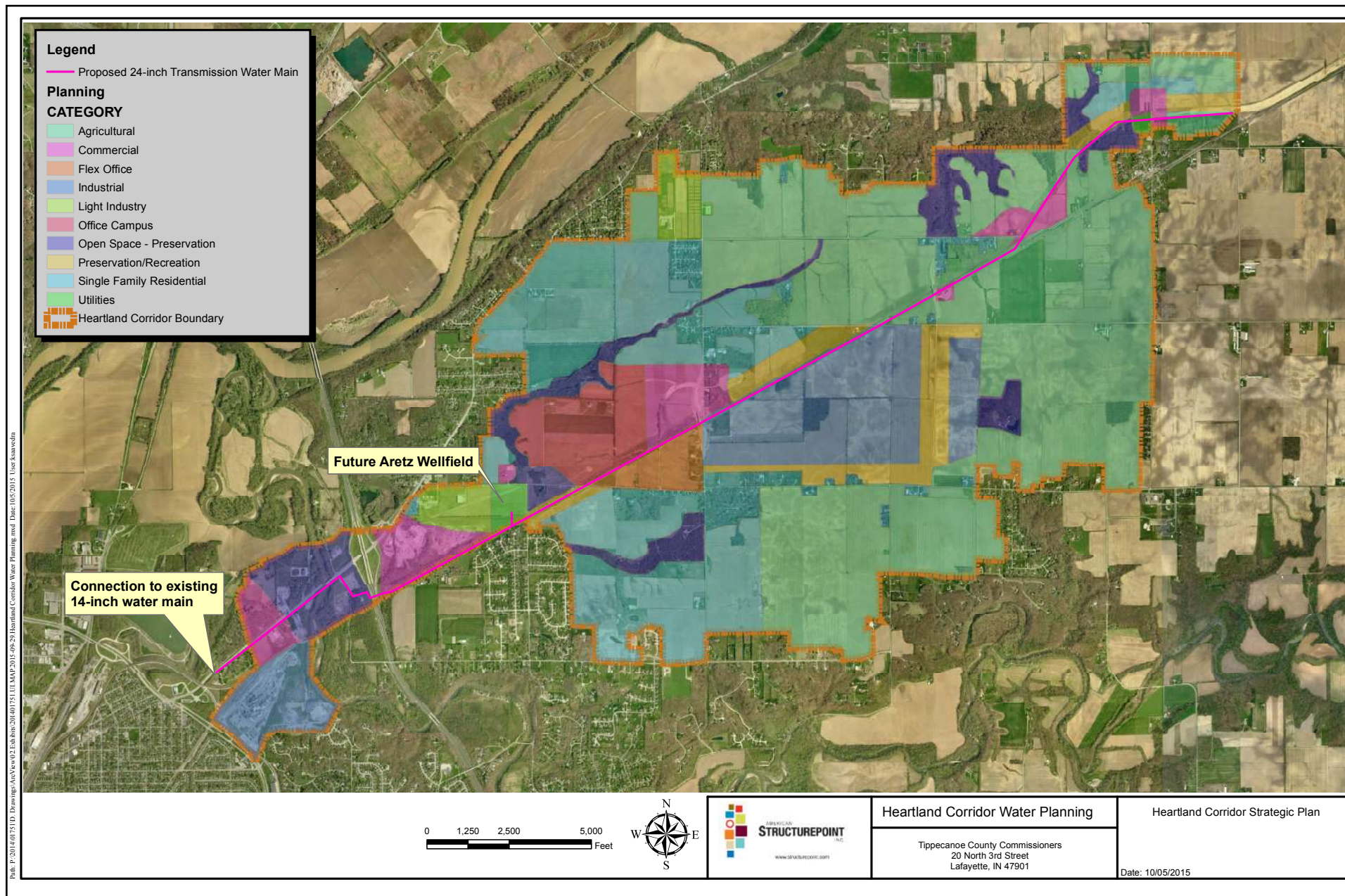
For drinking water, the City of Lafayette will develop the proposed Aretz wellfield with the 4-8MGD capacity listed in the report by the Service Area 21 Plan. The groundwater from the wells can be chlorinated and fluorinated and then distributed to the entire Heartland Corridor area using a 24-inch transmission water main. Loops can then be built off of this main. The main will also tie-in to an existing 14-inch water main in the City of Lafayette as a back-up. The pressure within the proposed 24-inch main is sufficient to supply the entire study area.

Policy Summary: *It is understood that developments beyond the currently allowed by zoning, small-scale residential developments, would require municipal utilities and services. Additional studies are required to fully determine the costs and strategy for utility infrastructure in the study area.*

Assumptions

1. There is 5-feet of cover over the pipe.
2. Valves will be installed every 1500 feet.
3. That connection can be made to the existing 14-inch water main on Schuyler Avenue.
4. That the Aretz well field will be developed by the City of Lafayette, and will produce 4 – 8 MGD of water with a pressure of at least 90 psi.
5. That the water demand was calculated using a flow factor of 500 gpd per Equivalent Dwelling Unit (EDU). EDU's were calculated based on projected wastewater flow for the proposed development divided by 310.

Proposed Water Infrastructure Map



Sanitary Sewer

Based on existing topography and planned land usage, it is recommended that a gravity “trunkline” sewer be installed in the right-of-way of the Heartland Corridor road. The sewer is sized to carry all of the flow from the Heartland Corridor study area. This major sewer will need to cross a small ditch, I-65, Wildcat Creek, and railroad tracks to tie-in to the closest existing sanitary sewer manhole. This existing manhole was suggested by the Service Area 21 Plan as the tie-in for the study area. The existing manhole currently receives flow from the Tate and Lyle North Plant and from a couple small businesses. The outlet from the existing manhole is a 24-inch pipe. Based on the proposed land uses for the Heartland Corridor area, the pipe at the downstream end needs to be a 30-inch pipe. Therefore, the sanitary sewer flow from the Heartland Corridor study area will not be able to flow into the existing Lafayette sewer. A study will need to be completed to determine where the proposed 30-inch sewer can flow to be able to reach the Lafayette Wastewater Treatment Plant.

Current residential uses within and immediately outside the study area were built with individual septic systems. In 1980, the unified zoning ordinance and the subdivision ordinance was refined so that new development will be required to connect to a sanitary sewer line. Expanding services to these areas will cost quite a bit but will eliminate the need for residents to maintain their own septic system and increase the tax base for the city.

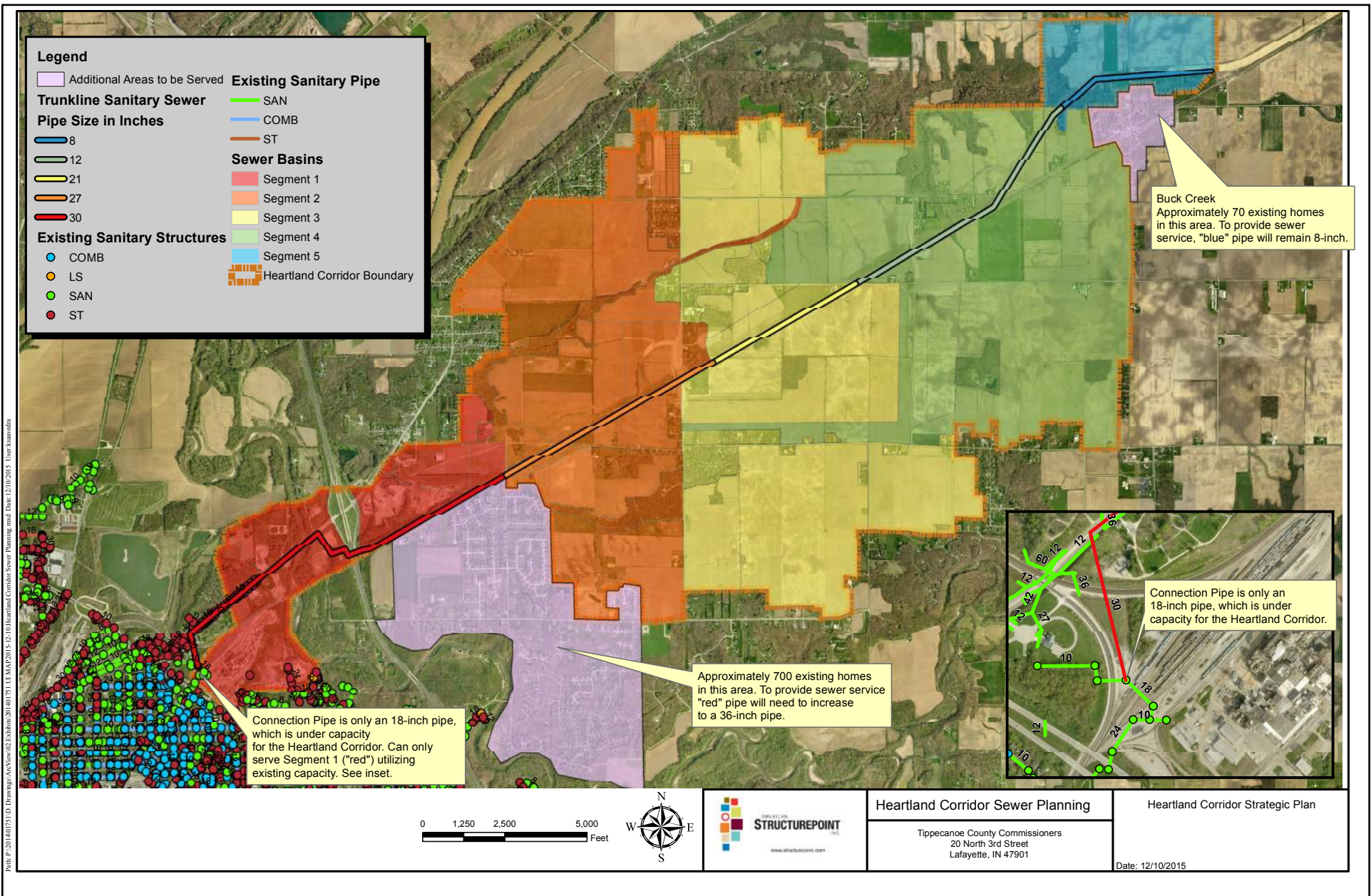
There are approximately existing 770 homes south of the Heartland corridor project area boundary that are currently using septic tanks for sanitary sewer treatment. Existing single family homes generate wastewater at the rate of 310 gallons per day per home; therefore, adding these additional homes will add 0.22 million gallons per day of wastewater flow to the system. The addition of these existing homes will cause the downstream sewer pipe leaving the Heartland Corridor area to increase from a proposed 30-inch pipe to a 36-inch pipe, which will further exacerbate the connection issues into the existing City of Lafayette wastewater sewer network.

Assumptions:

1. That there is adequate public right-of-way available for the installation of the sewer.
2. That 1/3 of the sewer will need to be installed under pavement, with the remaining 2/3 installed in native soil.
3. That the major trunkline sewer will, in general, follow Hoosier Heartland Highway.
4. That the sewer can connect by gravity to the existing manhole near the Tate and Lyle North plant.
5. That manholes will be installed every 400 feet along the route.
6. That the flow factors listed below represent the wastewater generated from each type of land use:
 - Land Use Flow Rate (gallons per day/acre)
 - Agricultural (50), Preservation (0), Utilities (0)
 - Industrial/Commercial/Office (750)
 - Single Family Residential - Existing (310)
 - Single Family Residential - Proposed (930)
7. No restoration of existing infrastructure (pavement, sidewalks, curb and gutter, etc.) is included.
8. Individual development projects will add branch sewers, manholes, and if necessary, small lift stations.
9. Based on the profile of the trunkline sewer, two lift stations will be needed – one to cross Wildcat Creek and the other to cross Buck Creek.
10. A casing pipe is included for all railroad and highway crossings.

Policy Summary: *It is understood that developments beyond the currently allowed by zoning, small-scale residential developments, would require municipal utilities and services. Additional studies are required to fully determine the costs and strategy for utility infrastructure in the study area.*

Proposed Sanitary Sewer Infrastructure Map



Stormwater

American Structurepoint reviewed Tippecanoe County's 2011 Comprehensive Stormwater Management Ordinance and Stormwater Technical Standards Manual to develop planning level estimates of stormwater detention requirements within the study area. A common rule of thumb within the site development field is that about 10% of a development area will need to be stormwater detention. This was used as a reference point, but it doesn't reflect the specific stormwater guidance and ordinances of Tippecanoe County or the site specific land uses, soils and terrain that ultimately dictate how much detention will be required.

The county stormwater ordinance prescribes allowable stormwater runoff release rates based on the soil types and conditions, terrain, and existing land use of the site. To stay below the allowable runoff rates in this study area, stormwater detention will be required. The amount of runoff produced depends on the proposed land use. For example, land uses with comparably more paved surfaces and buildings, such as industrial, will have higher runoff rates and require more stormwater detention. The proposed land uses in this study were divided into three categories based the expected runoff rates: high, medium, low. Each of these runoff categories was then evaluated individually to calculate a stormwater detention area specific to that expected runoff rate.

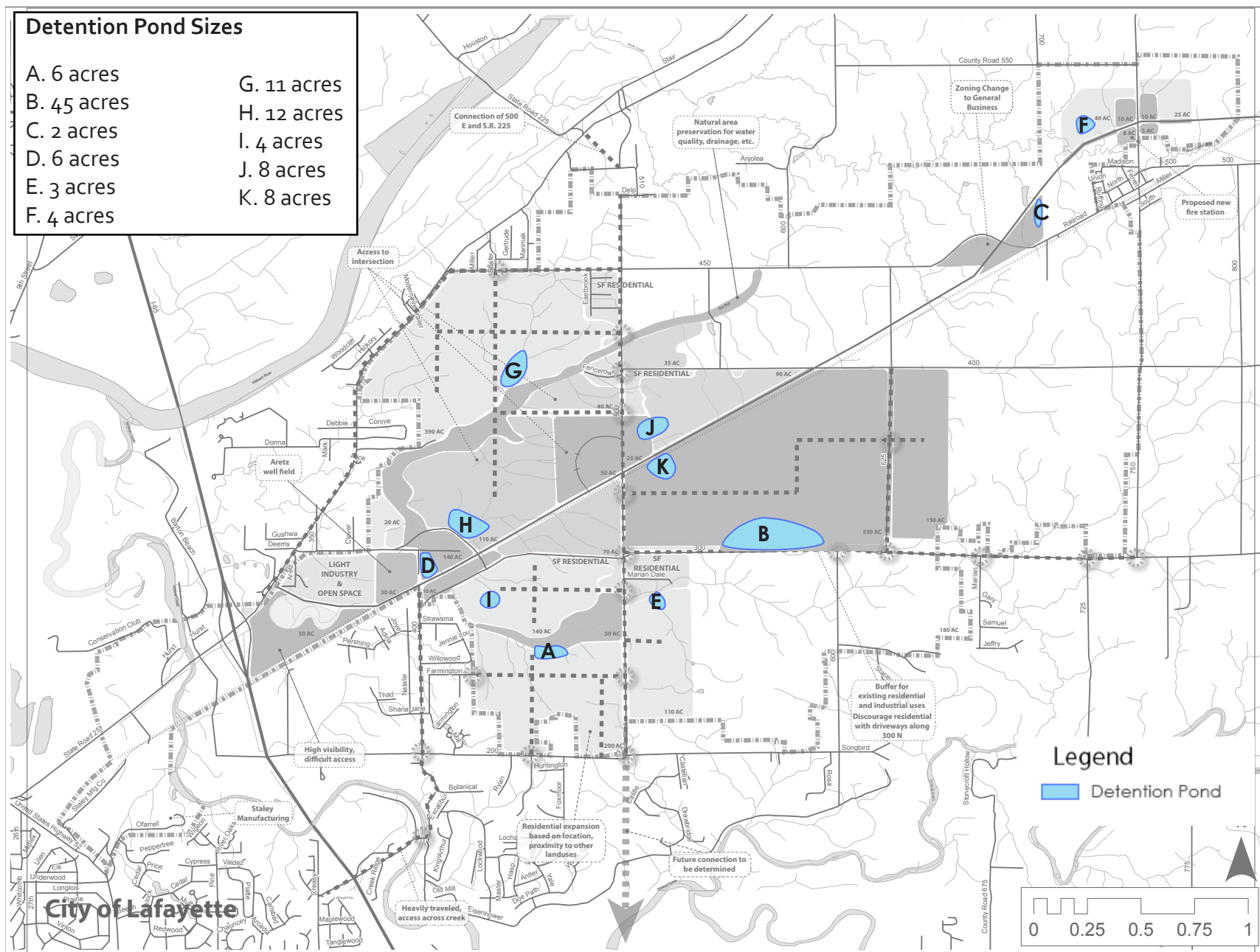
The results of this evaluation are presented as a percentage of the total percentage of each proposed land use type that will need to be used as stormwater detention. The detention area requirements provided are for planning purposes only. Design of stormwater detention basins, and their associated infrastructure, are site specific and will result in actual area requirements that vary from the planning estimate provided here. In addition to controlling runoff rates, stormwater detention designs will require set-back from existing buildings, security features, accessibility for maintenance, and grading and siting to fit the project site – all of which impact the overall land area required for detention.

ASSUMPTIONS:

- Pre-developed CN of 82
- Residential development at N 400 E and Farrington Dr. W used as example of proposed development (60 acres)
- 60 acres was used as the subbasin area for the analysis. Detention area was determined as a % of the subbasin area, so that it could then be applied to the larger proposed development areas in the Study.
- 4 vertical feet of available storage in detention basins
- The detention area is only an estimate of the detention water surface area at normal pool levels; additional land may be required for setback from buildings, security fences, site maintenance, grading, etc.
- The % detention area is only a planning level estimate. Final design will require site-specific design of individual detention basins, which could result basins larger and smaller than estimated here.

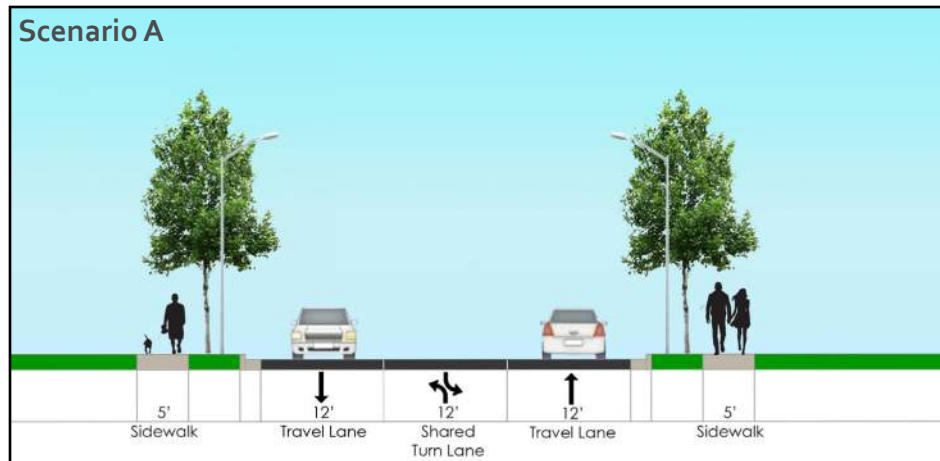
PROPOSED LAND USE CIENT % OF AREA AS	RUNOFF RATE DETENTION	RUNOFF COEFFI-
Single Family Residential 1.5%	Low	0.36
Multi-Family Residential 3.0%	Medium	0.54
Commercial 3.0%	Medium	0.54
Flex Office 3.0%	Medium	0.54
Office Campus 3.0%	Medium	0.54
Industrial 4.0%	High	0.63
Utilities 3.0%	Medium	0.54

Detention Pond Sizing/Locations Map (For illustration purposes only)



Existing Road Improvements

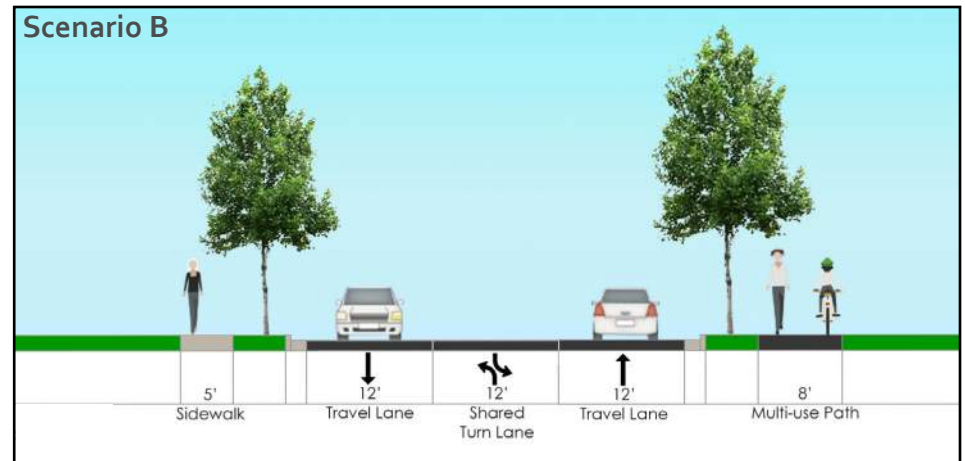
With the exception of the Hoosier Heartland, existing roads primarily serve local residents and low density development. Any development that occurs within the study area will bring an increased amount of traffic which conditions cannot support and are not planned for in the current transportation plans. Some roads will need to be upgraded in order to support additional traffic. The need for upgrades will be based on proposed future land uses and will accommodate alternative modes of transportation including pedestrians and bicyclists. Upgrades will provide improved access for existing users as well as provide necessary improvements for proposed land uses. All recommendations for existing roads in this plan will be reevaluated and adjusted every five years when the Area Plan Commission develops its Metropolitan Transportation Plans. Scenarios A, B, and C illustrate proposed cross sections for existing road segments. Cost estimates are general costs and do not include costs associated with utility relocation, property acquisition, and environmental impacts.



Cost Per Mile: \$4,774,000

Segment 1: 500E from 400N to Hoosier Heartland Corridor; three lane road with curb and gutter, sidewalks separated (5') on both sides, trees, and street lighting (.43 miles)

Cost estimate: \$2,573,000



Cost Per Mile: \$4,230,000

Segment 2: 500E from Hoosier Heartland Corridor to 300N; three lane road with curb and gutter, separated multi-use trail (8') on east side, separated sidewalks (5') on west side (.27 miles)

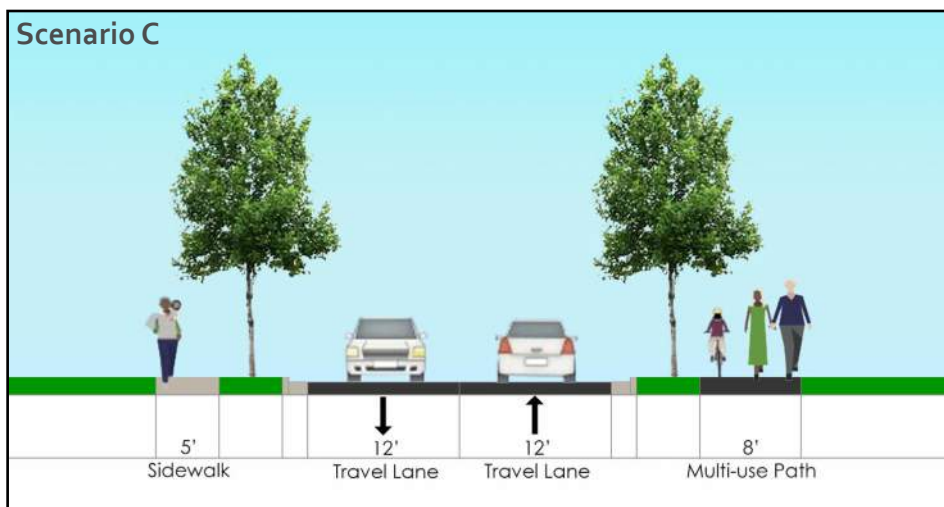
Segment 3: 500E from 300N to 200N; three lane road with curb and gutter, separated multi-use trail (8') on east side, separated sidewalks (5') on west side (1 mile)

Segment 6: 500E from 400N to 450N; three lane road with curb and gutter, separated multi-use trail on east side, separated (5') sidewalk on west side (.5 miles)

Cost estimate: Segment 2 - \$2,429,000

Segment 3 - \$5,799,000

Segment 6 - \$3,208,000



Cost Per Mile: \$3,411,000

Segment 4: 300N from 750E to 400E; two lane road with curb and gutter, add separated multi-use trail (8') on south side of road, separated sidewalk (5') on north side (3.6 miles)

Segment 5: 625E from 400N to 300N; two lane road with curb and gutter, separated multi-use trail (8') on east side and separated sidewalk (5') on west side (.93 miles)

Segment 7: 450N from 500E to Old S.R. 25; two lane road with curb and gutter, add separated multi-use trail (8') on south side of road, separated sidewalks (5') on north side (.87 miles)

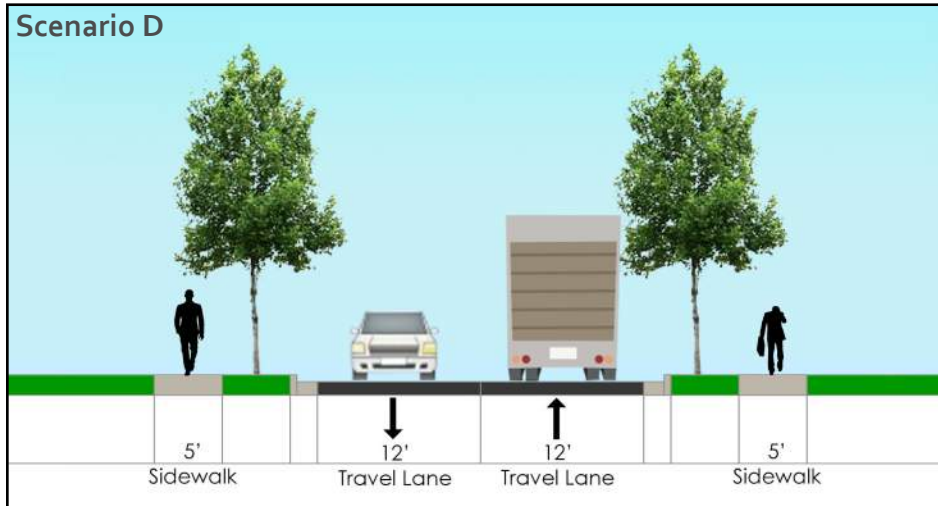
Cost estimate: Segment 4 - \$12,166,000

Segment 5 - \$4,317,000

Segment 7 - \$3,636,000

New Roads

This scenario is conceptual and is intended to illustrate the level of connectivity that is needed within and around future development areas and to show how new areas of development may potentially connect to surrounding areas. All recommendations for new roads in this plan will be reevaluated and adjusted every five years when the APC develops its Metropolitan Transportation Plans. Scenario D illustrates a proposed cross section for new road segments. Cost estimates are general costs and do not include costs associated with utility relocation, property acquisition, and environmental impacts.



Cost Per Mile: \$3,295,000

Segment 8: Access to multi-family residential commercial and office park areas; two lane road with curb and gutter, separated sidewalks (5') on both sides (1.03 miles)

Segment 9: Access into industrial site; two lane roads to accommodate heavy truck traffic with separated sidewalks (5') on both sides (1.93 miles)

Segment 10: Residential neighborhood network; two lane road with curb and gutter, separated (5') sidewalks on both sides (5.3 miles)

Corridor and Connectivity Studies

The overall transportation network in this area has existing challenges related to connectivity, traffic, and safety concerns specifically along Eisenhower Road, Old State Road 25, connections south and across Wildcat Creek and connections north to Old State Road 25. These road segments and lack of connections make various circulation patterns difficult given the amount of residents and traffic generated using the limited network. While the transportation challenges are clear, the solutions are not clear. It is recommended that for these various focus areas additional study be performed to clearly understand the issues, anticipate challenges with future development, and develop a solution that meets the needs of the current residents as well as future users. Solutions will be studied as part of the Metropolitan Transportation Plan every five years.

Segments

11. Eisenhower Road

Traffic along this road has increased and become problematic. The road is narrow, winding, and dangerous, and provides the only North-South connection over Wildcat Creek into Lafayette. Many residents use this access point to get into the city to eat, shop, and work as opposed to the Heartland Corridor and I-65.

12. 500E

A new road alignment should be studied to provide another access point across Wildcat Creek. With new development likely to occur at the 500E and Heartland Corridor intersection, and upgrades proposed to 500E, extending this road south of 200N would be an opportunity to carry more traffic across the creek into Lafayette. This extension would still provide a direct route into the city while also easing traffic along Eisenhower.

13. Old State Road 25 and Schuyler Avenue

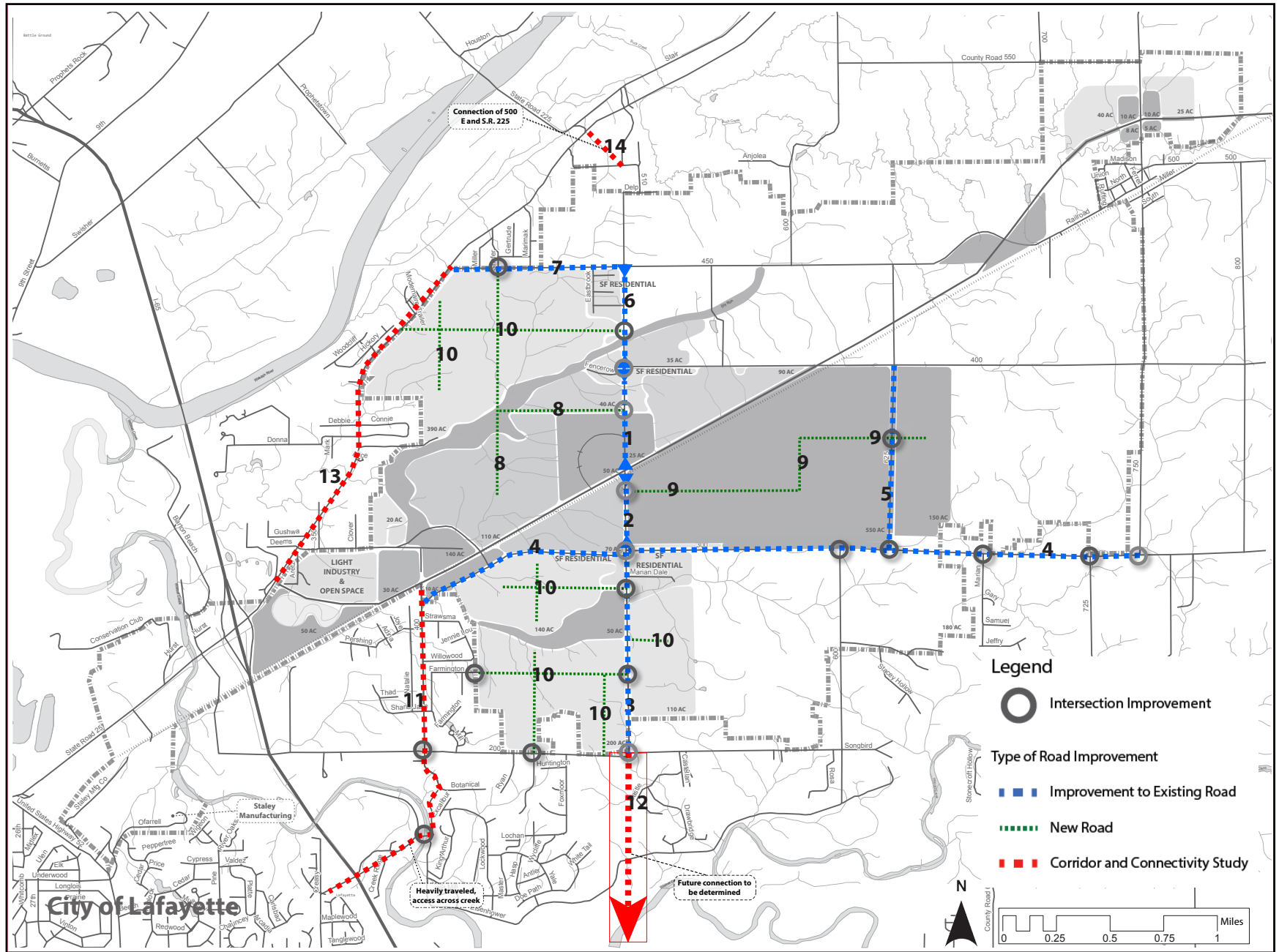
New development is likely to impact the segment of Old S.R. 25 on the west border of the study area. Proposed single family residential in the immediate

area and commercial along the Hoosier Heartland Corridor will increase traffic along this portion of Old S.R. 25 and is recommended to be upgraded to accommodate this.

14. Alignment of S.R. 25 to 500E

A new alignment to connect these roads will provide more direct access to areas north of the study area including Prophetstown State Park and Battleground.

Proposed Transportation Improvements Map



Economic Development Tools

Property Tax Caps

In the 2010 November election, voters passed property tax caps. Residential property taxes are now limited to one percent of the assessed value, secondary residential properties and agricultural land may not exceed two percent of the assessed value, and other real estate (business and industrial) are capped at three percent, per Indiana State statute. This new tax structure limits the money available to local government, but it is intended to offer 'stability' to property owners. Property taxes are the "primary source of funding for local government units, including counties, cities and towns, townships, libraries, and other special districts, including fire districts, public transportation, and solid waste districts." (<http://www.in.gov/dlgf>) The tax cap limits the funds and makes it essential to diversify the tax base and capture taxes from higher-rated uses such as commercial and industrial.

Tax Abatement

Tax abatement is a tool commonly used throughout Indiana. Many communities find it is by far the most competitive bargaining component when trying to attract business. Tax abatement of all or a portion of taxes on a new or increased assessed value resulting from new investment can be granted for up to ten years. Tax abatement can be applied to real or personal property and provides a relief for the property owners who are improving and reinvesting in their property. Using tax abatement within a TIF district should be thoroughly understood and used selectively. While the use of tax abatement within a TIF district may be counterproductive to the district since it will delay the contribution to the TIF fund, tax abatement may be the best strategy from time to time and should be considered by thoroughly evaluating the pros and cons. For large industrial development, this incentive will be low on the site selection list.

Municipal Bonds

A municipal bond is a security that is issued by local government to raise funds primarily for the purpose of financing infrastructure needs of the county such

as streets, highways, bridges, sewer and water systems, schools, hospitals, jails, fire and police stations, power utilities, and various other public projects. Put simply, bonds are a form of loan where an IOU is provided by an issuer or borrower to a lender or holder of the bond. The holder of the bond provides the funds necessary to complete the project. The issuer owes the holder a debt and is typically required to pay interest at set intervals and to repay principal at maturity. The interest or debt service is paid through general obligations of the issuer or secured by specified revenues, special assessments, or a combination thereof. While there are other potential sources, the primary revenue sources to pay the debt service are property taxes, local option income taxes, or tax increment revenues.

Tax Increment Finance District

Generally, a TIF district enables local government officials to collect incremental property tax revenue from increased assessed value, resulting from new investments within the designated area. When a TIF is designated, a base assessed value (current level) is recorded. When there is an increase in real and/or depreciable personal property assessed value, the difference or increase in property tax revenue is set aside in a specific fund to be reinvested in the district on infrastructure or other improvements, including the potential to invest in capital or training needs for potential primary employers. This gives the community a specific funding stream to attract and encourage private investment that may not occur without the local support.. The property taxes attributed to the base assessed value are collected and distributed to the traditional taxing units (schools, city, township, and county) as with all other property taxes. Those funds are used according to the allocations and structure set forth by the town, typically for schools, improvements, and other services. The allocations and structure of TIF districts are annually reviewed by the Redevelopment Commission and approved by the County Commissioners. These allocations specify how TIF monies can be used. TIF revenue is primarily

used to invest directly into projects or as leverage to cover debt service on a municipal bond as previously discussed.

While the philosophy of TIF can certainly be debated, it is a proven success factor in the communities where it is implemented, it is an expectation of virtually every business site selection effort, and it is no longer viewed as a differentiator between communities but rather a critical standard component of a community's competitiveness. For example, if a company is interested in a piece of property but the property is not connected to necessary infrastructure, it may be beneficial for the county to use funds from the TIF to pay for these improvements if the anticipated assessed value will repay the fund with the incremental taxes due to increased assessed value. These improvements benefit the initial development as well as subsequent businesses and reduce costs for future businesses.

Local Financing

Working with local banks to establish preferred finance programs for potential business investors is a great economic development tool. This establishes a relationship and a favorable rate and assists potential developers as well as supports the local financial institution. Most importantly, a difficult step is made easier, thus making Tippecanoe County a more likely choice for business selection and location. These rates will need to have parameters and expectations defined prior to any utilization of them to ensure the financing tool supports the overall vision for this area.

Shovel Ready Program

The Shovel Ready Program through the Indiana Office of Community & Rural Affairs, (OCRA) designates and certifies sites that are ready for development. There are three levels of Shovel Ready including "Shovel Ready", Shovel Ready Silver", and Shovel Ready Gold". Base level designation requires clear boundaries with a title, price, government support, and clear utility capacity. Silver level requires all the previous attributes with documentation less than a year old, proper zoning, and infrastructure built to the property. Gold designation requires being less than 5 miles from a two-lane highway, seismic

data, soil borings, no environmental concerns, and minimum 20 acres. Seeking shovel ready designation for sites in the area is a great way to market available land for developers. Sites that are designated are featured on Indiana's Site Selector Database and is included in the Indiana Economic Development corporation's marketing materials.

Public Private Partnerships

Public-private partnerships (referred to as PPP or P3) are a creative way to combine assets to achieve desired economic development improvements. Public-private partnerships are joint ventures between public and private sectors. "In their best form, public-private partnerships reduce risk, secure development capital at reasonable rates and provide a high return on investment for taxpayer and developer alike. Communities can effectively meet the needs of their residents while otherwise sub-optimized commercial resources are efficiently put to work." (Smith, Thomas. Inside INdiana Business, 2010). The collaboration of the two different entities offers expertise, resources, and opportunity. Building strong public/private partnerships between Tippecanoe County and the regional development community will create opportunities to understand perceptions, clarify misconceptions, and clearly articulate the expectations of the county as it relates to development within its jurisdiction and move forward products for development.

Impact Fees

Impact fees are payments required by local governments of new development for the purpose of providing new or expanded public capital facilities required to serve that development. The fees typically require cash payments in advance of the completion of development, are based on a methodology and calculation derived from the cost of the facility and the nature and size of the development, and are used to finance improvements off site of, but to the benefit of, the development. Local governments throughout the country are increasingly using impact fees to shift more of the costs of financing public facilities from the general taxpayer to the beneficiaries of those new facilities. The fees supplement local government resources that otherwise have decreased because of diminished state and federal transfers of funds. Impact

fees, when based on a comprehensive plan and used in conjunction with a sound capital improvement plan, can be an effective tool for ensuring adequate infrastructure to accommodate growth where and when it is anticipated. It is important that communities rely on zoning and other land use regulations, consistent with a comprehensive plan, to influence patterns of growth and to more accurately predict new infrastructure needs. Indiana statute permits impacts fees to finance public infrastructure projects such as sanitary sewer systems, water or wastewater treatment facilities, parks or recreation facilities, roads and bridges, and/or flood control facilities.

Site and Design Standards

The vision for the physical character of this area can be generally described as focused on detail, proportion, and quality, as well as being traditional and conservative. To maintain this identity when new development or redevelopment opportunities arise, it is important to apply these characteristics to the new developments in some form. A set of predefined development standards or design guidelines are often a helpful tool for both the county's economic development team and the potential developer to understand the expectations of the community. This will allow them to deliver the desired development outcomes. Standards should be enforced universally and consistently on all developers, users, or investors within the area to ensure that all previous and future investments are protected. Consistent enforcement and a well-defined and efficient development review process are strongly encouraged. These design standards and enforcement guidelines are outlined in Tippecanoe County's UZO and USO. These standards should be continually referred to as growth and development occur.

The following general guidelines are recommended to be applied to the entire study area. All development and infrastructure within the 2010 adjusted urban area boundary is required to use urban standards and follow the adopted complete streets policy. The physical buildings and infrastructure in higher density development are encouraged to use principles that support pedestrian connectivity and pedestrian scale, as well as proper pedestrian and urban

proportions. A key design principal is to pay as much attention to the pedestrian and the cyclist as is paid to the car. A balanced approach to transportation adds to the quality of life and place that is created. The buildings are encouraged to be relatively close to the right-of-way and maintain a consistent frontage and building edge. This will ensure that, while each piece may be done independently or at different time, the thematic overall building form and massing desired by the community can be maintained. In pedestrian-oriented commercial areas, a continuous "street wall" of façades should be created. Easy access from parking areas to the shopping street, entrances, or plaza is encouraged.

Commercial, industrial, office campus, and flex office uses are encouraged to be master planned in conjunction with adjacent similar uses to facilitate efficient circulation and shared parking. Where developments include several buildings, grouping of buildings to create outdoor spaces and plazas is encouraged. Open space should be located contiguous to open space on adjacent sites to maximize their combined visual effect. A straightforward and visually pleasant approach to building entrances is also encouraged. Residential uses are also encouraged to be master planned and developed in neighborhoods rather than individual, isolated units. Residences with driveways along county roads should be discouraged. Sensitivity to adjacent uses should be a high priority for all uses, and buffers and transition areas should be utilized to minimize the adverse effects of incompatible uses.

Streetscapes should also reflect quality and detail. Building edges and adjacent land uses will be a factor in designing streetscape elements. In general, the streetscape should consist of unified elements, including street lights, street trees, shrubs, perennial plantings, sodded parkways, continuous sidewalks, pedestrian crossings, pavement markings, and bicycle facilities where appropriate.

High-quality building design and construction is desired on all elevations (360-degree architectural treatment), with the exception of predetermined areas that are internally visible. Architectural detailing should be focused on the building entry and on façades visible from any roadway. The scale, mass, color, and proportion of the building should reflect the character of the area in which it is located and should be compatible with adjoining developments.

Multiple buildings in developments (including accessory buildings and detached out-lot buildings in commercial centers, where permitted) must incorporate coordinated architectural styles, materials, forms, features, colors and applied elements to visually tie the development together. Signage, fencing, walls, and other amenities (benches, lights) are encouraged to be integrated with building design and landscaping. All structures will be evaluated on the overall appearance of the project and should be based on the quality of its design and its relationship to the surrounding area.

Buffering land uses will be an important part of developing in this area. Landscaping, mounding, and fencing should be utilized to hide undesirable uses and protect the scenic character of the area. Specific land uses can also be used to buffer development. For example, multi-family residential development can be used to buffer single family and commercial or industrial uses. High quality methods of buffering allow undesirable but strategic land uses to develop here while maintaining and preserving the scenic character of the area. Design standards and requirements for buffering are located in the Tippecanoe County UZO.

IMPLEMENTATION TOOLS

Critical Path Strategies

Critical Path Strategies

The critical path strategies are the most essential strategies in achieving the vision and goals set forth by this plan. All of the recommendations are important, but the critical path strategies are actions that should be initiated and completed first. Completing these first will allow for further growth and opportunities to arise.

	CRITICAL PATH STRATEGY	PRIORITY - START TIME	TIME TO COMPLETE	PRIMARY RESPONSIBLE PARTY	SECONDARY RESPONSIBLE PARTY	OUTSIDE RESOURCE
1	Conduct studies to determine next steps and phasing for sanitary sewer expansion into the study area.	HIGH - NOW	OVER 12 MONTHS	City of Lafayette, Area Plan Commission	County Health Department	Professional consultant
2	Conduct studies to determine next steps and phasing for water utility expansion into the study area.	HIGH - NOW	OVER 12 MONTHS	City of Lafayette, Area Plan Commission		Professional consultant
3	Conduct studies to determine improvements, new connections, and priority projects within the existing vehicular circulation framework .	HIGH - NOW	OVER 12 MONTHS	Tippecanoe County, Area Plan Commission	City of Lafayette	Professional consultant
4	Evaluate the tax increment finance district boundary for parcels to be added or removed based on the proposed land use plan and the goals of the strategic plan.	HIGH - SOON	LESS THAN 12 MONTHS / ONGOING	Tippecanoe County Redevelopment Commission, Area Plan Commission	City of Lafayette	None
5	Review, discuss, draft, and adopt regulatory standards for quality development and maintenance of scenic character.	HIGH - SOON	LESS THAN 12 MONTHS / ONGOING	Area Plan Commission	City of Lafayette, Greater Lafayette Commerce	Professional consultant
6	Develop a low-impact development stormwater management plan to evaluate impacts of stormwater runoff and develop best practices.	HIGH - SOON	OVER 12 MONTHS	Area Plan Commission	Tippecanoe County Partnership for Water Quality	Professional consultant
7	Evaluate available economic development tools to promote growth and encourage private investment within the study area.	HIGH - SOON	LESS THAN 12 MONTHS / ONGOING	Greater Lafayette Commerce	City of Lafayette, Tippecanoe County Commissioners, Area Plan Commission	Professional consultant

1. Conduct studies to determine next steps and phasing for *sanitary sewer* expansion into the study area.

Providing adequate sanitary sewer infrastructure is one of the most critical components of encouraging increased investment and development within the study area and one of the first steps toward implementing this plan. Without this infrastructure, agriculture and rural development will continue to be the extent of investment that occurs in this area. Existing residents and businesses currently are serviced with separated septic tanks because necessary infrastructure from the City of Lafayette stops west of Interstate 65.

The Service Area 21 Plan recommended a connection to the closest existing sanitary pipe west of the interstate, but this recommended connection is currently at capacity. In addition, the level and intensity of development recommended in this plan has increased significantly from the land use plan that was used to guide the recommendations in the Service Area 21 Plan. Given these issues outlined on pages 44-45, a study should be conducted to determine the best, most cost effective solution to handle the additional demand that will be generated by future development. Predicted flow rates from development based on the proposed land uses will exceed the current capacity of infrastructure in the area, so other options will need to be evaluated to handle and treat the water.

South of, and adjacent to, the study area, there are many existing homes being serviced by septic tanks. There should be a coordinated effort involving the Tippecanoe County Health Department to evaluate these systems and determine if and when there might be a need to connect these homes into the future sanitary sewer infrastructure based on the condition of the septic tanks.

Priority
High

Start Time
Now

Time to Complete
Over 12 months

Primary Responsible Party
City of Lafayette
Area Plan Commission

Secondary Responsible Party
County Health Department

Outside Resource
Professional consultant

2. Conduct studies to determine next steps and phasing for *water utility* expansion into the study area.

Development of the Heartland Corridor will have impacts on the demand for water supply and how the area is serviced. The City is developing the Aretz Wellfield which will have sufficient capacity to provide water for the study area. An evaluation of the area's future demands and needed infrastructure should be conducted. This should include a review of the Service Area 21 Plan in order to determine necessary updates in anticipation of the Hoosier Heartland Corridor's Strategic Land Use and Economic Development Plan's recommendations. The predicted land uses laid out in the Service Area 21 Plan represent a much smaller area and at a much lower intensity than the recommendations included in this plan. The water CAD model created in that plan was based on the dated land use plan, and should be updated to account for the full study area and the larger area that is being recommended. The modeling will help to determine if there is adequate water pressure to service the entire study area.

Water Quality Analysis studies should also be conducted to ensure a safe water supply. Coordinate with the city to have wells drilled specifically for testing and include this information in the city's water quality report.

Priority
High

Secondary Responsible Party

Start Time
Now

Outside Resource
Professional consultant

Time to Complete
Over 12 months

Primary Responsible Party
City of Lafayette
Area Plan Commission

3. Conduct studies to determine improvements, new connections, and priority projects within the existing *vehicular circulation framework*.

Existing road infrastructure in the study area is aging and, in some places, has deteriorated to the point where it is barely capable of handling existing traffic from rural residential and agricultural uses. Future planned development will not only stress existing infrastructure further, but will also eventually result in demand that necessitates increases in capacity. A more detailed analysis of the future traffic demands should be completed in order to determine impacts on the existing system and provide recommendations for appropriate solutions.

The Proposed Transportation Improvements Map on page 52 highlights four road segments that recommend additional Corridor and Connectivity Studies. Each of these transportation corridors or potential future corridors are important routes to and from the Heartland Corridor study area and would provide improved connectivity to other critical areas of the community. Each of these routes will require different levels of improvement in order to handle the increased traffic demand that will be generated as future development occurs. A study should be conducted for these road segments to determine alternatives that address this need in the most appropriate manner.

The Proposed Transportation Improvements Map also recommends transportation solutions that include both new road segments and upgrades to existing roads to increase capacity in order to meet the expected future demand. These recommendations are extensive and will need to be prioritized based on critical needs and strategic development sites. Once critical areas have been identified and road segments prioritized, a capital plan should be completed that includes estimated construction budgets. This work should include a more detailed survey of the study area and proposed alignments and cross sections for each of the road segments. Once this work is completed, a financial plan can be produced and funding sources can be identified. However, other than high priority or strategic projects that should be completed in advance of development to encourage private investment, it is anticipated that road projects will primarily be completed based on the demand and/or need generated by development.

Priority
High

Secondary Responsible Party
City of Lafayette

Start Time
Now

Time to Complete
Over 12 months

Outside Resource
Professional consultant

Primary Responsible Party
Tippecanoe County
Area Plan Commission



4. Evaluate the *tax increment finance district boundary* for parcels to be added or removed based on the proposed land use plan and the goals of the strategic plan.

In 2014, the TIF district boundary in Tippecanoe County along the Hoosier Heartland Corridor was created based on existing land uses. The current single-family residential uses were excluded from the TIF district because tax increment cannot be collected from that land use.

However, the proposed land use plan recommends some current single family residential properties to become another land use as future development occurs. In this case, these parcels should be added back into the TIF district to collect the incremental increase in assessed value. All parcels currently in the TIF district should be reevaluated based on their existing and proposed land uses. Parcels should continue to be reevaluated as development and other changes occur to maximize the amount of tax increment revenue generated from this district.

Areas outlined in light blue in the map on page 63 are ones that should be considered for adding into the TIF district. These areas were not originally included because of the existing single-family residential land use, but are proposed to be commercial, office campus, and multi-family residential users. Adding the parcels in these highlighted areas into the TIF district before development occurs will ensure the increase in assessed value and resulting tax increment is collected for the benefit of the district.

Priority
High

Start Time
Soon

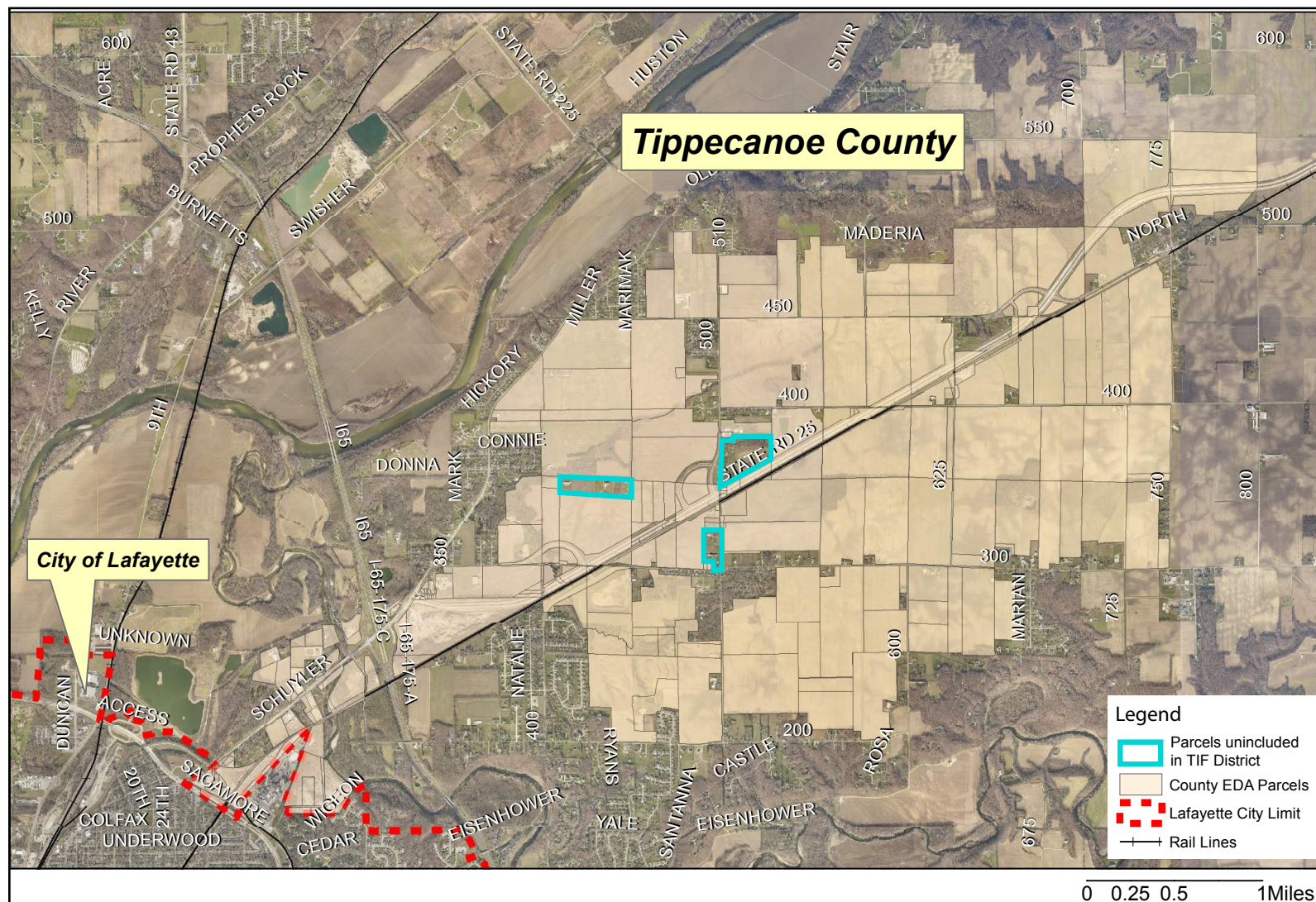
Time to Complete
Less than 12 months / on-going

Primary Responsible Party
Tippecanoe County Commissioners
City of Lafayette

Secondary Responsible Party
Area Plan Commission

Outside Resource
(none)

TIF Boundary vs. Proposed Land Use Analysis



5. Review, discuss, draft, and adopt *regulatory standards* for quality development and maintenance of scenic character.

Development in the study area will need to be carefully planned and monitored for high quality design, materials, and appropriate size and density. In order to ensure that the vision developed in this plan is carried out as investments occur, appropriately detailed development and architectural standards need to be in place.

The Tippecanoe County Area Plan Commission currently has design standards in their UZO and UDO for setbacks, buffering, landscaping, etc. These standards have been the county's guiding recommendations for development for many years. The APC should conduct a detailed evaluation of the current standards to ensure that they will result in the high quality product and character that the current and future residents, visitors, and potential businesses will desire, as well as fulfilling what's being recommended in this plan. If deficiencies are determined, then appropriate updates and additions should be developed and adopted into the current standards.

Priority
High

Start Time
Soon

Time Frame
Less than 12 months

Primary Responsible Party
Area Plan Commission

Secondary Responsible Party
City of Lafayette, Greater Lafayette Commerce

Outside Resource
Professional Consultant



6. Develop a low-impact development *stormwater management plan* to evaluate impacts of stormwater runoff and develop best practices.

New development in the study area will heavily impact stormwater runoff. In an effort to reduce the impacts on the County's utility system, this area should utilize innovative and comprehensive strategies to efficiently and effectively treat and discharge stormwater runoff. A Low Impact Development Stormwater Plan should be created to identify the threats to water quality in the area and propose solutions that can be implemented for the study area.

In partnership with the Tippecanoe County Partnership for Water Quality, stakeholders will develop a comprehensive understanding of existing site conditions, environmental concerns, and threats to water quality. Together project stakeholders will identify a series of stormwater and sustainability goals for future development as well as a series of best management practices that can be implemented across the site. The contents of the plan should include detailed information such as design specifics, construction requirements, costs and post installation monitoring opportunities. The plan should also address any necessary grey infrastructure improvements for the area.

In addition to overall guidelines for the area, the plan can identify smaller pilot projects that can be undertaken to further the education and understanding of low impact development and stormwater management solutions in the area.

Priority
High

Start Time
Soon

Time Frame
Over 12 months

Primary Responsible Party
Area Plan Commission

Secondary Responsible Party
Tippecanoe County
Partnership for Water
Quality

Outside Resource
Professional Consultant



7. Evaluate available *economic development tools* to promote growth and encourage private investment within the study area.

Utilizing strategic economic development tools is essential to encouraging the growth and development of the study area. A TIF district has already been established as a mechanism to reinvest tax revenue within the area primarily in the form of infrastructure improvements and additional project incentives. The economic development team in Tippecanoe County and the City of Lafayette should evaluate the various methods to leverage TIF revenue and determine those strategies that would be most effective in this area. Each strategy has a unique level of risk, but, if used in a proactive manner, the potential for reward in the form of private investment may be realized in a shorter timeframe. The appetite for risk should be determined in order to put the most impactful strategy in place to create sites that are “ready” for development. There are many other economic development tools to be exercised that will help promote and market the area. Examples of tools and their benefits are explained on pages 53-54.

In addition to an investment strategy, the economic development team should also determine goals and a plan for marketing the study area. The marketing strategy should further identify potential market segments, the appropriate audience, and methods to reach that audience.

Priority
High

Start Time
Soon

Time Frame
Less than 12 months / on-going

Primary Responsible Party
Greater Lafayette
Commerce

Secondary Responsible Party
City of Lafayette
Tippecanoe County
Commissioners
Area Plan Commission

Outside Resource
Professional Consultant

(THIS PAGE INTENTIONALLY LEFT BLANK)

APPENDIX A

Public Meeting Materials

PRINCIPLES & VISION

- TOP 6

19

Encourage balanced, phased development to protect the study area's natural resources and environment.

18

Emphasize preservation of farmland and natural areas.

18

Focus on creating a quality place to live, work, and play.

12

Commit to a long-term vision while developing within the study area over time.

10

Understand and respond to the study area's potential impact on surrounding communities.

10

Concentrate on developing views and vistas and on the experience of the resident, visitor, and business owner.
- 7

Create a gateway to Lafayette from I-65 at the Heartland Corridor.

6

Retain and expand existing businesses while pursuing opportunities to attract new businesses to the study area.

4

Focus new development in areas with respect to context and access to infrastructure.
- 4

Expand transportation and utilities infrastructure to meet increased demand from future phased development.

4

Foster communication and partnerships among local government, residents, and businesses.

4

Promote site availability and readiness for phased development.

3

Create a gateway to new development within the study area.

3

Capitalize on regional assets and connections.

2

Utilize a variety of economic development tools, (tax increment financing, tax abatement, and local financing) to incentivize development within the study area.

1

Pursue development in phases that creates jobs and increases tax revenue within the study area.

1

Pursue opportunities to develop public-private partnerships.

*Participants asked to pick TOP 5 most important principles and vision

VISUAL PREFERENCE SURVEY - RESPONSES & INPUT

AGRICULTURE

Fit?	Priority?	Visual Avg.
21 Yes 0 No	17 Yes 2 No	3.95
Small Independent Farms		

Fit?	Priority?	Visual Avg.
21 Yes 0 No	17 Yes 2 No	4.05
Traditional, Large Crop Farming		

Fit?	Priority?	Visual Avg.
15 Yes 4 No	9 Yes 8 No	3.20
Agribusiness		

Fit?	Priority?	Visual Avg.
4 Yes 17 No	2 Yes 17 No	1.42
Traditional, Large Scale Animal Farming		

*Participants took a survey and were asked based on each picture,

1. would it fit in the study area?
2. should it be a priority for the study area?

INDUSTRIAL

Fit?	Priority?	Visual Avg.
11 Yes 10 No	6 Yes 12 No	3.02
Office/Technology Park		

Fit?	Priority?	Visual Avg.
8 Yes 12 No	2 Yes 15 No	2.37
Warehousing/Distribution Center		

Fit?	Priority?	Visual Avg.
2 Yes 19 No	1 Yes 17 No	1.42
Heavy Energy Production		

Fit?	Priority?	Visual Avg.
6 Yes 15 No	6 Yes 12 No	2.09
Manufacturing		

3. Rate the picture on a scale of 1-5 (5 being best) based on,
 - structure/design
 - materials
 - size/scale

RESIDENTIAL

Fit?	Priority?	Visual Avg.
19 Yes 2 No	14 Yes 2 No	4.57
Single Family Rural Homes		

Fit?	Priority?	Visual Avg.
18 Yes 3 No	13 Yes 4 No	3.94
Single Family Suburban Homes		

Fit?	Priority?	Visual Avg.
7 Yes 14 No	3 Yes 14 No	2.61
Medium Density Townhomes/Duplexes		

Fit?	Priority?	Visual Avg.
1 Yes 20 No	1 Yes 16 No	1.87
High Density Apartments/Condominiums		

COMMERCIAL

Fit?	Priority?	Visual Avg.
10 Yes 10 No	9 Yes 9 No	2.63
Auto-Oriented Strip Mall		

Fit?	Priority?	Visual Avg.
10 Yes 10 No	6 Yes 11 No	2.81
Centralized, Walkable Commercial		

Fit?	Priority?	Visual Avg.
11 Yes 9 No	5 Yes 12 No	2.56
Neighborhood Commercial Node		

Fit?	Priority?	Visual Avg.
4 Yes 16 No	1 Yes 17 No	1.70
Regional Shopping Center		

RECREATIONAL

Fit?	Priority?	Visual Avg.
16 Yes 4 No	9 Yes 9 No	3.09
Sports Complex		

Fit?	Priority?	Visual Avg.
11 Yes 9 No	4 Yes 14 No	2.77
Golf Course		

Fit?	Priority?	Visual Avg.
15 Yes 5 No	7 Yes 10 No	3.20
Centralized Play Space		

Fit?	Priority?	Visual Avg.
4 Yes 16 No	1 Yes 17 No	1.58
Event Space/Performance Venue		

NATURAL RESOURCES/OPEN SPACE

Fit?	Priority?	Visual Avg.
17 Yes 3 No	11 Yes 6 No	3.40
Green Infrastructure/Bioswales		

Fit?	Priority?	Visual Avg.
18 Yes 2 No	10 Yes 6 No	3.51
Multi-use Trails		

Fit?	Priority?	Visual Avg.
19 Yes 1 No	11 Yes 5 No	4.16
Passive Open Space		

Fit?	Priority?	Visual Avg.
20 Yes 0 No	15 Yes 2 No	4.50
Natural Preservation		

VEHICULAR TRANSPORTATION

Fit?	Priority?	Visual Avg.
11 Yes 7 No	8 Yes 9 No	3.19
Landscaped Avenues		

Fit?	Priority?	Visual Avg.
20 Yes 0 No	17 Yes 1 No	4.16
Collaboration for Farming Operations		

Fit?	Priority?	Visual Avg.
10 Yes 10 No	9 Yes 8 No	2.59
Roundabout Intersection		

Fit?	Priority?	Visual Avg.
6 Yes 14 No	5 Yes 12 No	2.12
Complete Streets		

PEDESTRIAN TRANSPORTATION

Fit?	Priority?	Visual Avg.
13 Yes 7 No	11 Yes 7 No	3.07
Sidewalks		

Fit?	Priority?	Visual Avg.
11 Yes 7 No	5 Yes 11 No	3.00
No Sidewalks		

Fit?	Priority?	Visual Avg.
15 Yes 5 No	12 Yes 6 No	3.71
Bicycle Lanes		

Fit?	Priority?	Visual Avg.
17 Yes 3 No	12 Yes 5 No	3.78
Multi-use Trails		

UTILITIES INFRASTRUCTURE

Fit?	Priority?	Visual Avg.
13 Yes 6 No	11 Yes 6 No	2.86
Power Lines		

Fit?	Priority?	Visual Avg.
6 Yes 13 No	5 Yes 12 No	2.10
Wind Turbines		

Fit?	Priority?	Visual Avg.
11 Yes 7 No	8 Yes 9 No	3.03
Solar Panels		

Fit?	Priority?	Visual Avg.
13 Yes 6 No	10 Yes 7 No	3.55
Regional Retention Pond		

VISUAL CHARACTER

Fit?	Priority?	Visual Avg.
4 Yes 16 No	0 Yes 17 No	1.56
Billboard Signage		

Fit?	Priority?	Visual Avg.
20 Yes 0 No	16 Yes 2 No	4.30
Rural Character and Scenic Views		

Fit?	Priority?	Visual Avg.
10 Yes 9 No	7 Yes 11 No	3.25
Design Guidelines/Consistency		

Fit?	Priority?	Visual Avg.
11 Yes 9 No	9 Yes 9 No	2.51
Building Variety/Freedom of Design		

QUALITY OF LIFE

Fit?	Priority?	Visual Avg.
19 Yes 1 No	14 Yes 3 No	2.51
Transportation and Safety		

Fit?	Priority?	Visual Avg.
13 Yes 7 No	12 Yes 6 No	3.08
Jobs and Employment		

Fit?	Priority?	Visual Avg.
19 Yes 1 No	16 Yes 2 No	4.55
Environmental Responsibility		

Fit?	Priority?	Visual Avg.
13 Yes 7 No	10 Yes 8 No	2.56
Diverse and Quality Housing		

COMMUNITY IDENTITY

Fit?	Priority?	Visual Avg.
20 Yes 0 No	16 Yes 2 No	4.67
Unique Local Qualities		

Fit?	Priority?	Visual Avg.
5 Yes 15 No	2 Yes 15 No	2.21
Regional Attraction		

Fit?	Priority?	Visual Avg.
6 Yes 14 No	5 Yes 12 No	2.36
Community Branding		

Fit?	Priority?	Visual Avg.
9 Yes 10 No	5 Yes 11 No	2.60
Community Programming/Organizations		

Agriculture

Support for small, traditional farms (existing)

Mixed responses for agribusiness

Dislike large scale animal farms, concern with CAFOs affecting quality of life

Industrial

Mixed responses for office/technology parks and warehousing/distribution

Dislike heavy energy production and manufacturing, concern with aesthetics and pollution

Residential

Support for rural and suburban homes (existing), concern with sprawl

Dislike higher density housing like townhomes, apartments, condominiums, etc.

Commercial

Mixed responses for auto, pedestrian, and neighborhood shopping

Dislike large regional shopping

Recreational

Support for sports complexes and centralized play spaces

Mixed responses for new golf course

Dislike event spaces

Natural Resources/Open Space

Support for all preservation and open space uses.

Vehicular Transportation

Support for collaboration with farmers

Mixed responses for roundabouts and landscaping streets

Dislike complete streets

Pedestrian Transportation

Support for bike lanes and multi-use trails

Mixed responses for presence of sidewalks and absence of sidewalks

Utilities Infrastructure

Mixed responses for powerlines, solar panels, and retention ponds

Dislike wind turbines

Visual Character

Support for rural character and views (existing)

Mixed responses for consistent design and building design variety

Dislike billboards along Heartland and other areas

Quality of Life

Support for transportation/safety and environmental responsibilities

Mixed responses for jobs/employment and diverse, quality housing

Community Identity

Support for unique local qualities

Mixed responses for branding and programming/organizations

Dislike regional attractions

LAND USE SCENARIO CONCEPT SUMMARIES

Concept #1

Public Comments Summary

1. Residents prefer the study area's current rural setting over new commercial, industrial, and residential uses.
2. If developed, commercial and industrial uses should be appropriately buffered from all conflicting uses.
3. Create a high quality gateway from I-65 and Lafayette.
4. Office and technology park development should be concentrated closer to Lafayette and around the I-65 interchange.

Justification

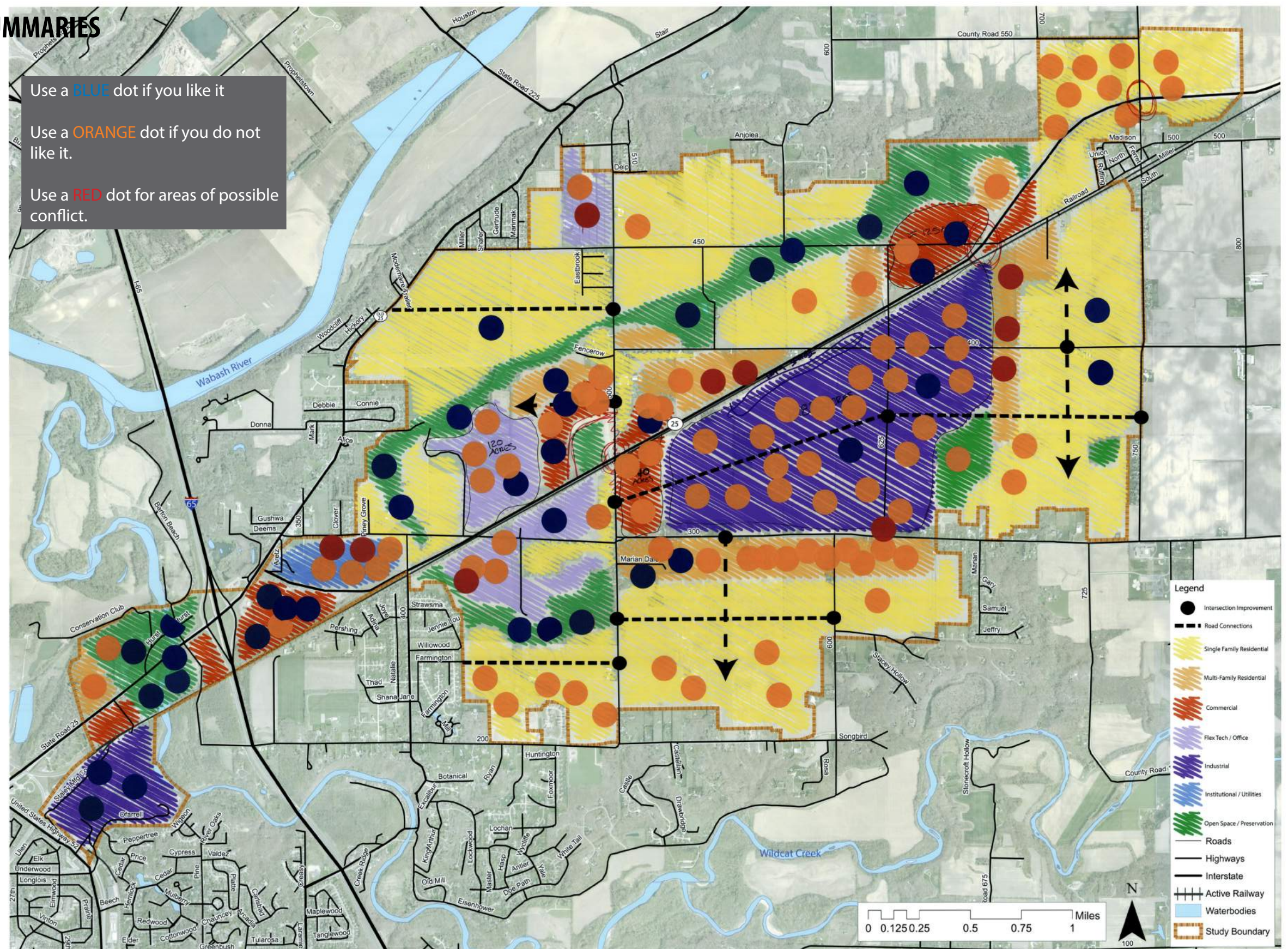
Prime land for industrial users in this area includes access to rail, I-65, and SR 25, and full access interchanges. Buffering land uses or natural features will help separate low and high intensity uses.

Commercial uses are supported by I-65 and SR 25 and can also be buffered from residences.

Higher density residential uses create tax revenue for TIF, while single family does not.

A short distance from Lafayette and visibility from I-65 creates an ideal area for gateway commercial uses.

Land north of SR 25 offers ideal land for because of access to SR 25 and close proximity to Lafayette and Purdue.



LAND USE SCENARIO CONCEPT SUMMARIES

Concept #2

Public Comments Summary

- 1. Residents prefer the preservation of agricultural land uses.
- 2. Commercial uses should be focused around the I-65 interchange, Lafayette, and Buck Creek.
- 3. Natural areas should be maintained and expanded for wildlife preservation and recreational opportunities.
- 4. Continue support for existing low density uses to maintain the study area’s rural character.

Justification

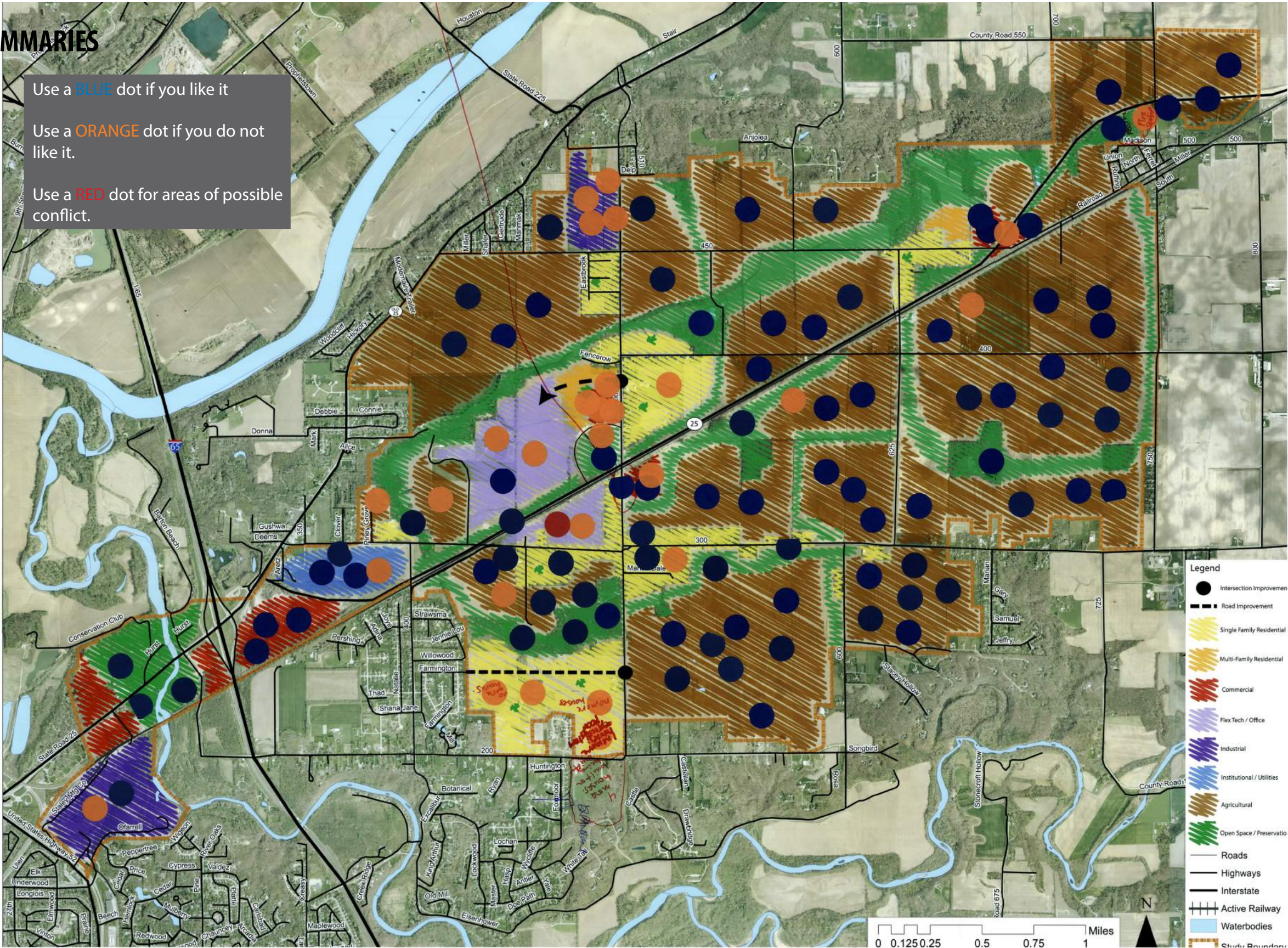
Agricultural preservation should remain a high priority for the study area; however, new development also be pursued to increase tax revenue.

Areas close to interchanges will support quality commercial uses, zoning changes along SR 25 now allow general business.

Supporting a “green network” and natural areas provide for wildlife to thrive and enhance rural character.

Higher density residential uses create tax revenue for TIF while single family does not.

Promoting low density development encourages sprawl and high infrastructure costs while generating less tax revenue.

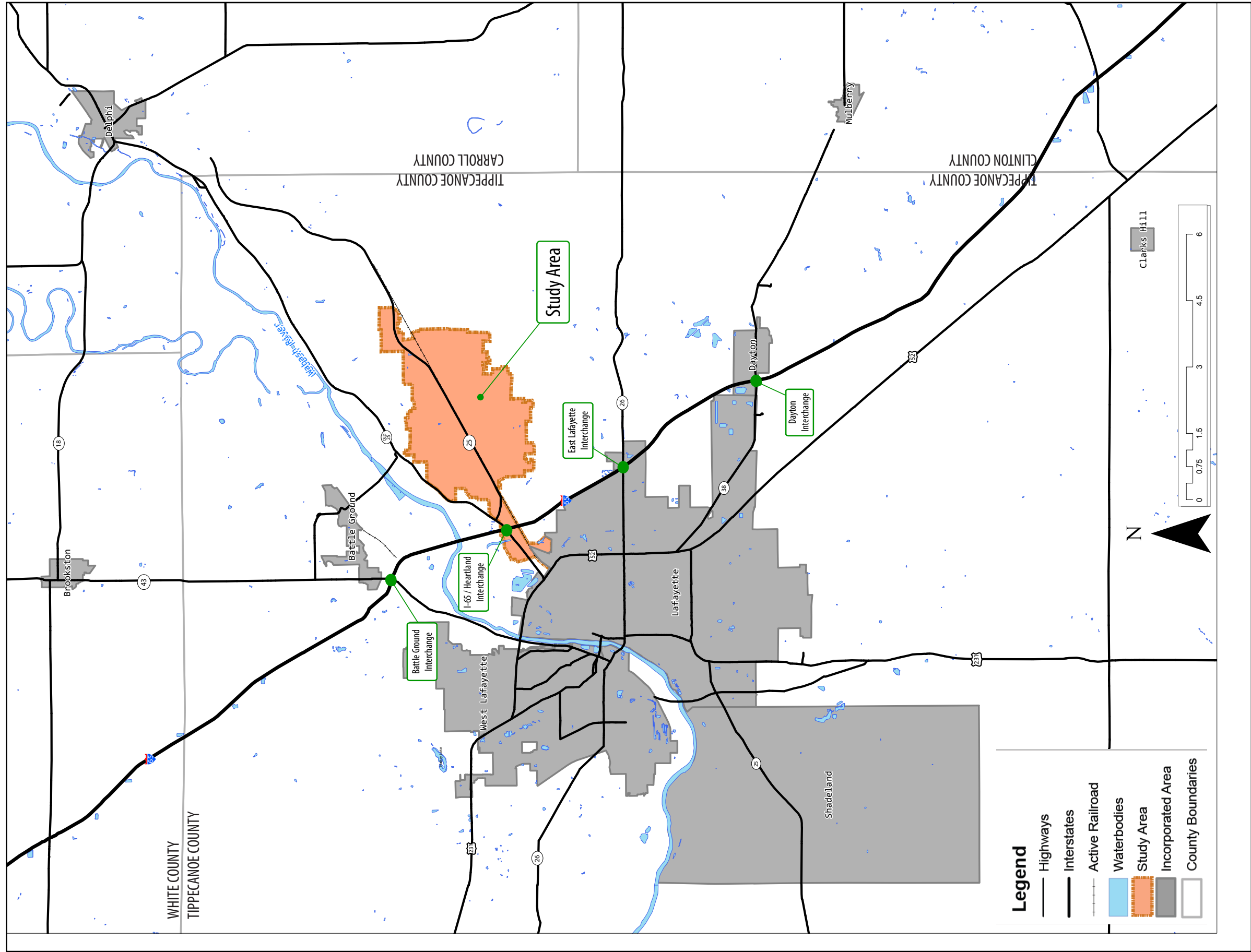


APPENDIX B

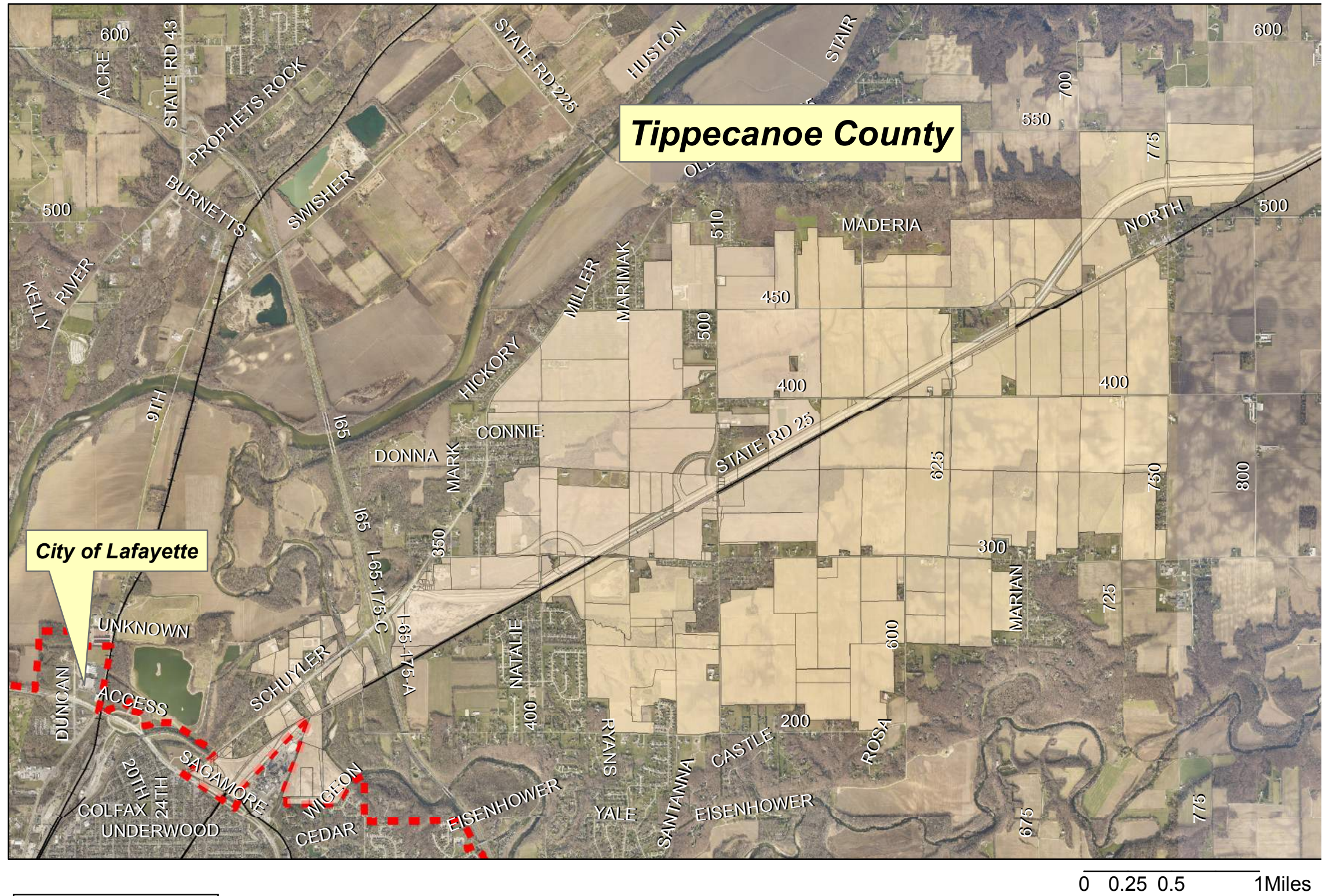
Large Versions of Maps

(THIS PAGE INTENTIONALLY LEFT BLANK)

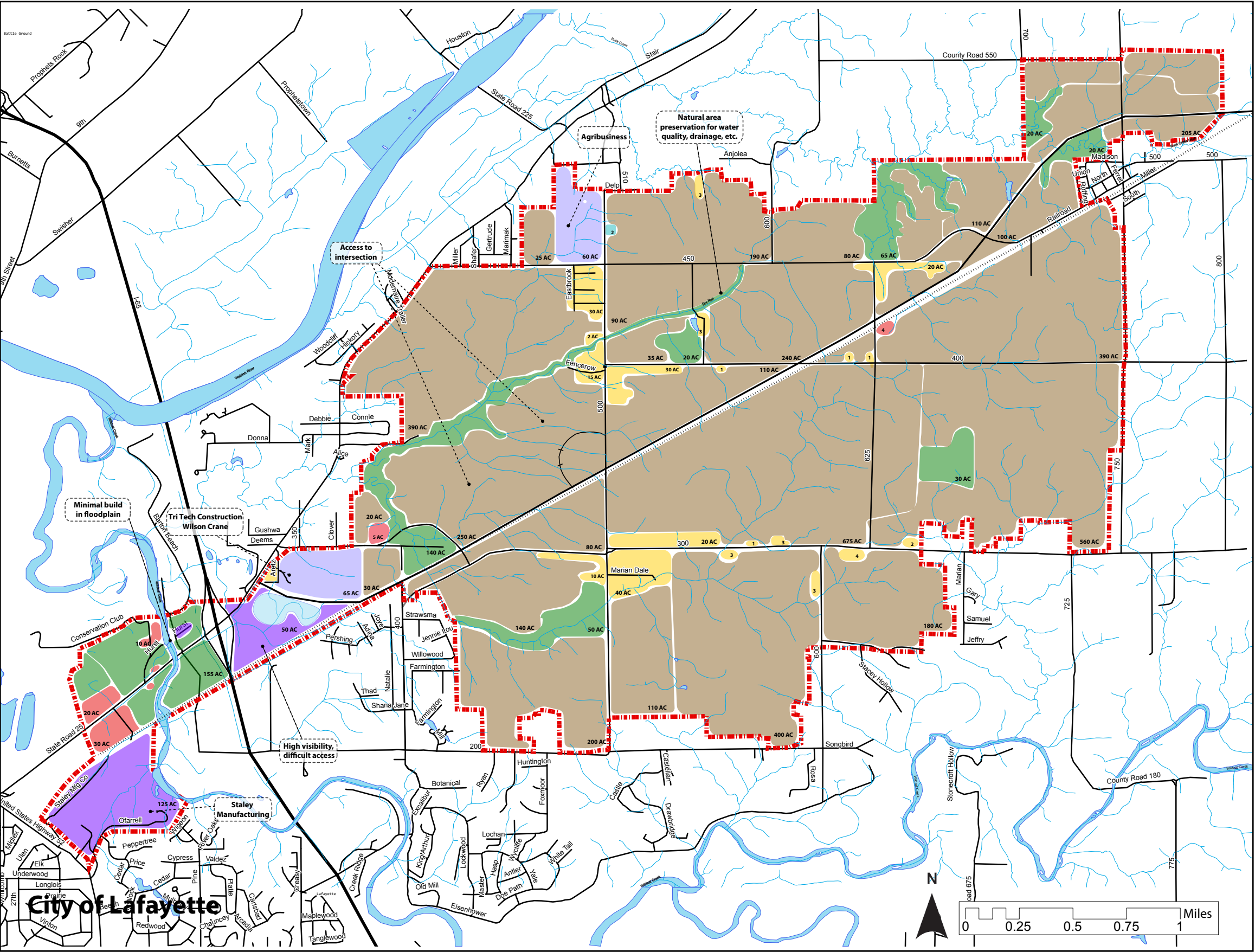
Context Map



Study Area TIF Boundary



Existing Land Use Map

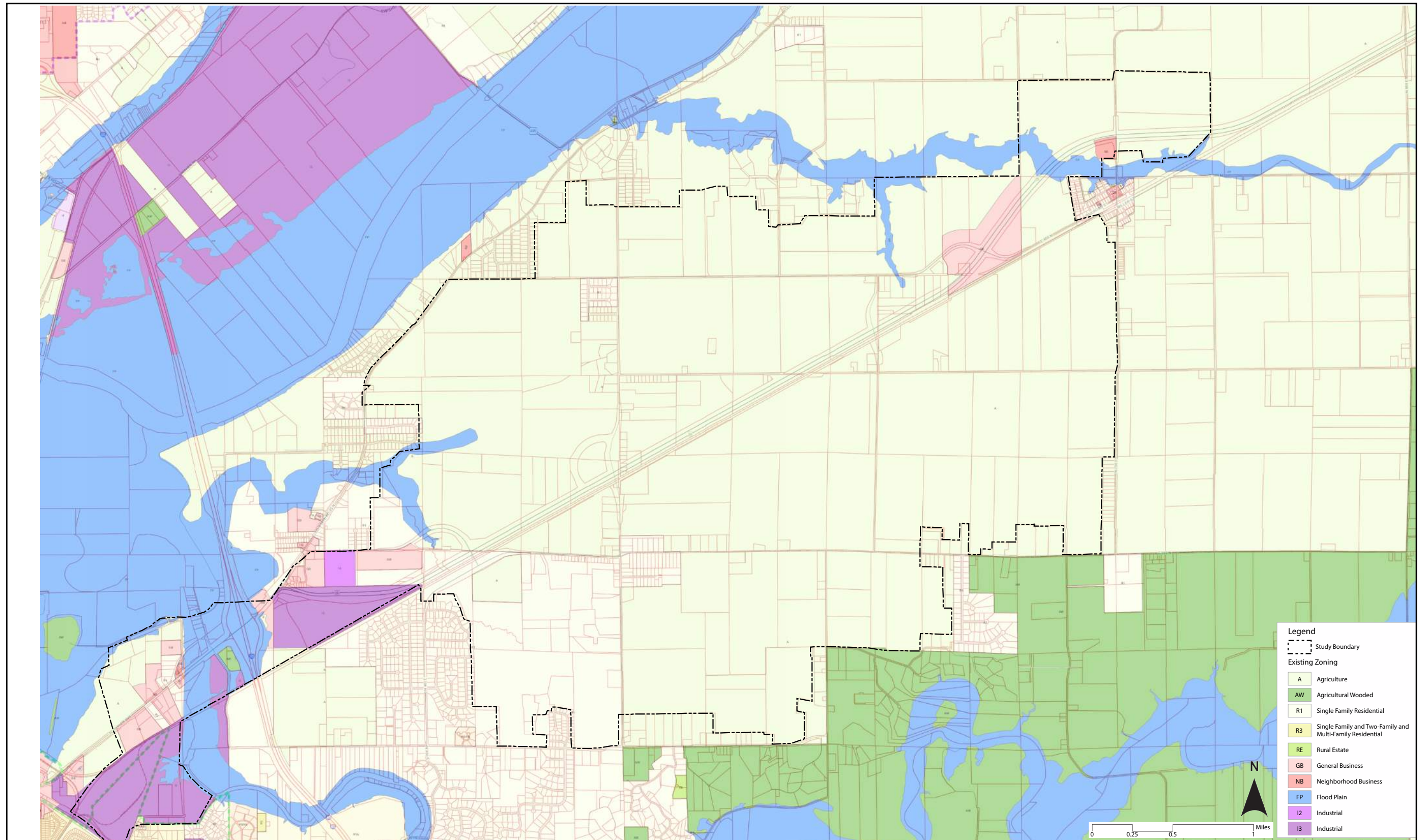


Legend

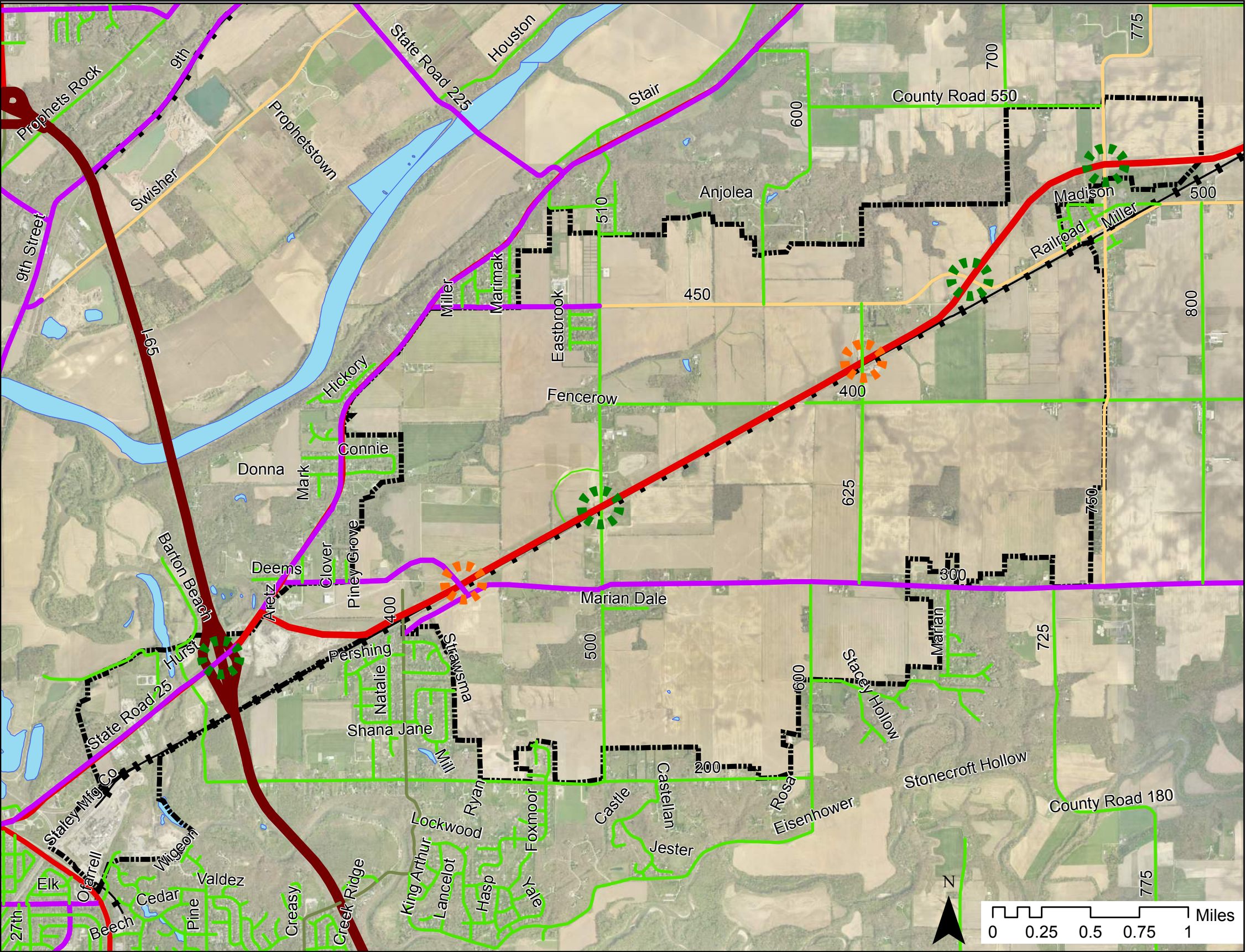
Existing Land Uses and Features

- Single Family Residential
- Commercial
- Light Industry
- Industrial
- Preservation / Recreation
- Utilities
- Agricultural
- Streets
- Drainage
- Waterbodies
- Study Boundary

Existing Zoning Map



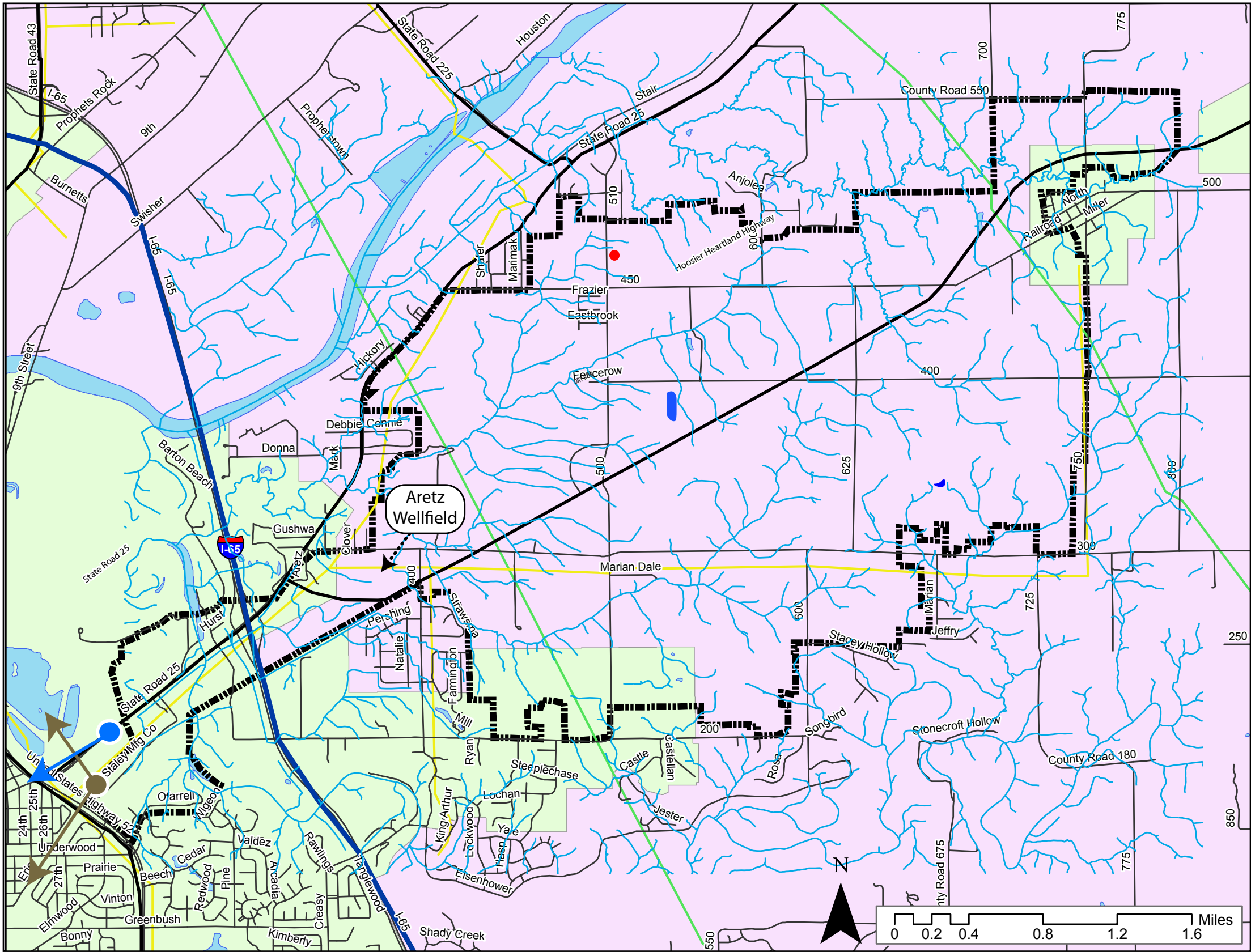
Functional Classification Map



Legend

- Overpass
- Full Access Intersection
- Functional Classification
 - Interstate
 - Local
 - Major Collector
 - Minor Collector
 - Principal Arterial
- Major Collector
- Waterbodies
- Study Boundary

Circulation Infrastructure Map



Legend

●

End of Existing Sanitary Sewer

●

End of Existing Water Main

←

Existing Sanitary Sewer

←

Existing Water Main

Electric Service Territory

■

Duke Energy

■

Tipmont REMC

●

Substation

■

Detention Pond

—

Drainage

—

Overhead Power Lines

—

Natural Gas

—

Refined Products

—

Roads

—

Highways

—

Interstate

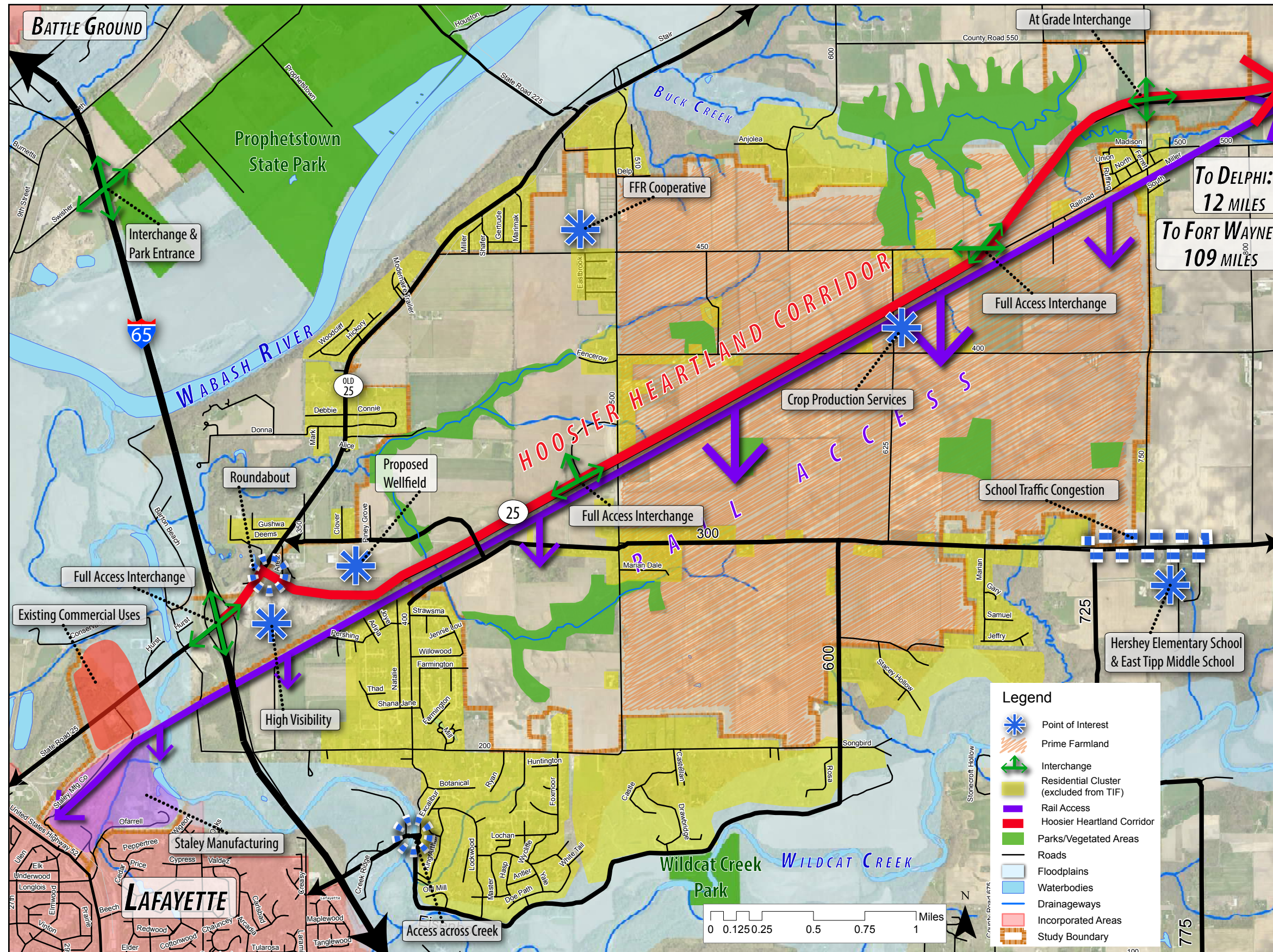
■

Waterbodies

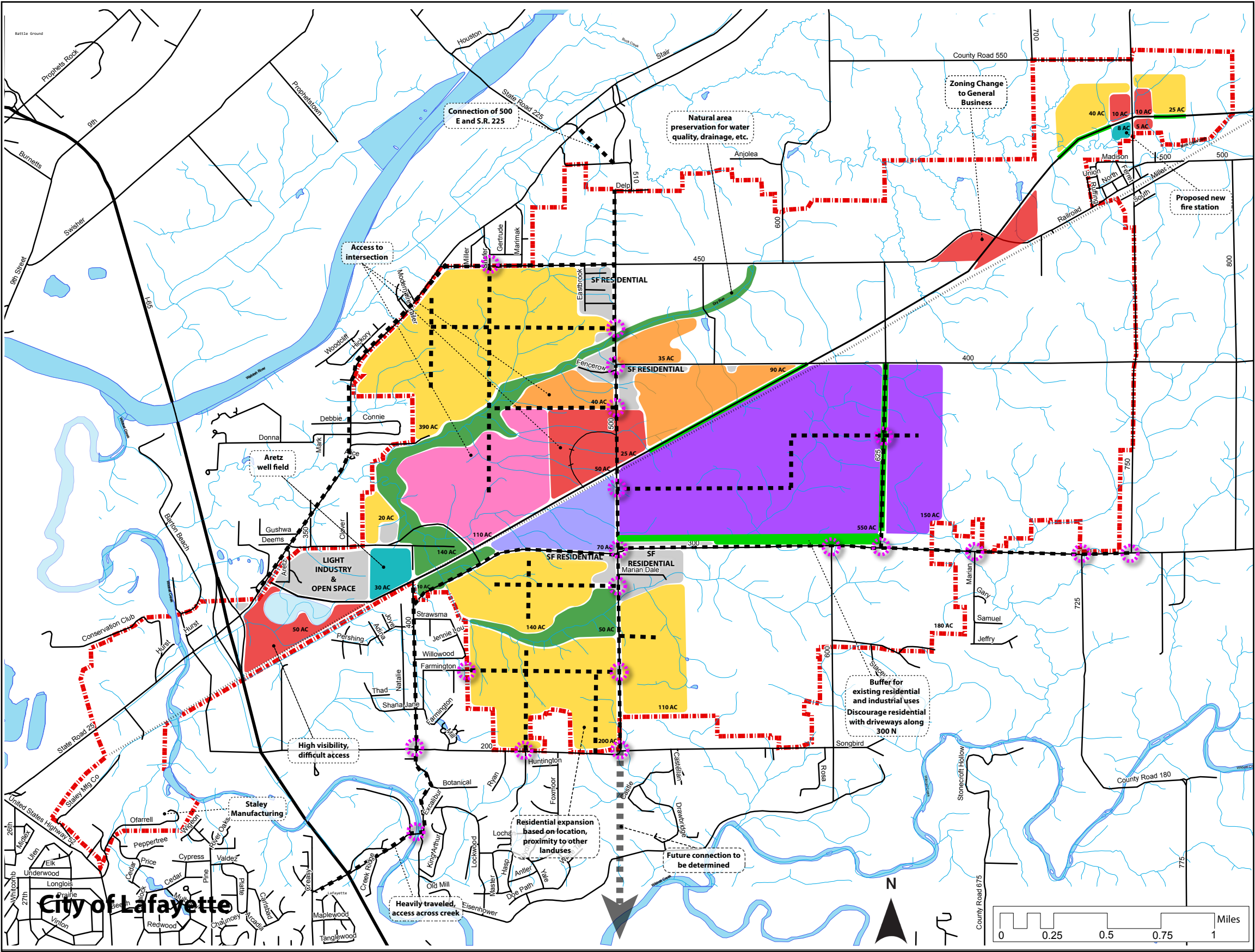
■

Study Boundary

Study Area Analysis Map



Proposed Land Use Map



Legend

Recommendations

- Road Improvements
- Intersection Improvements

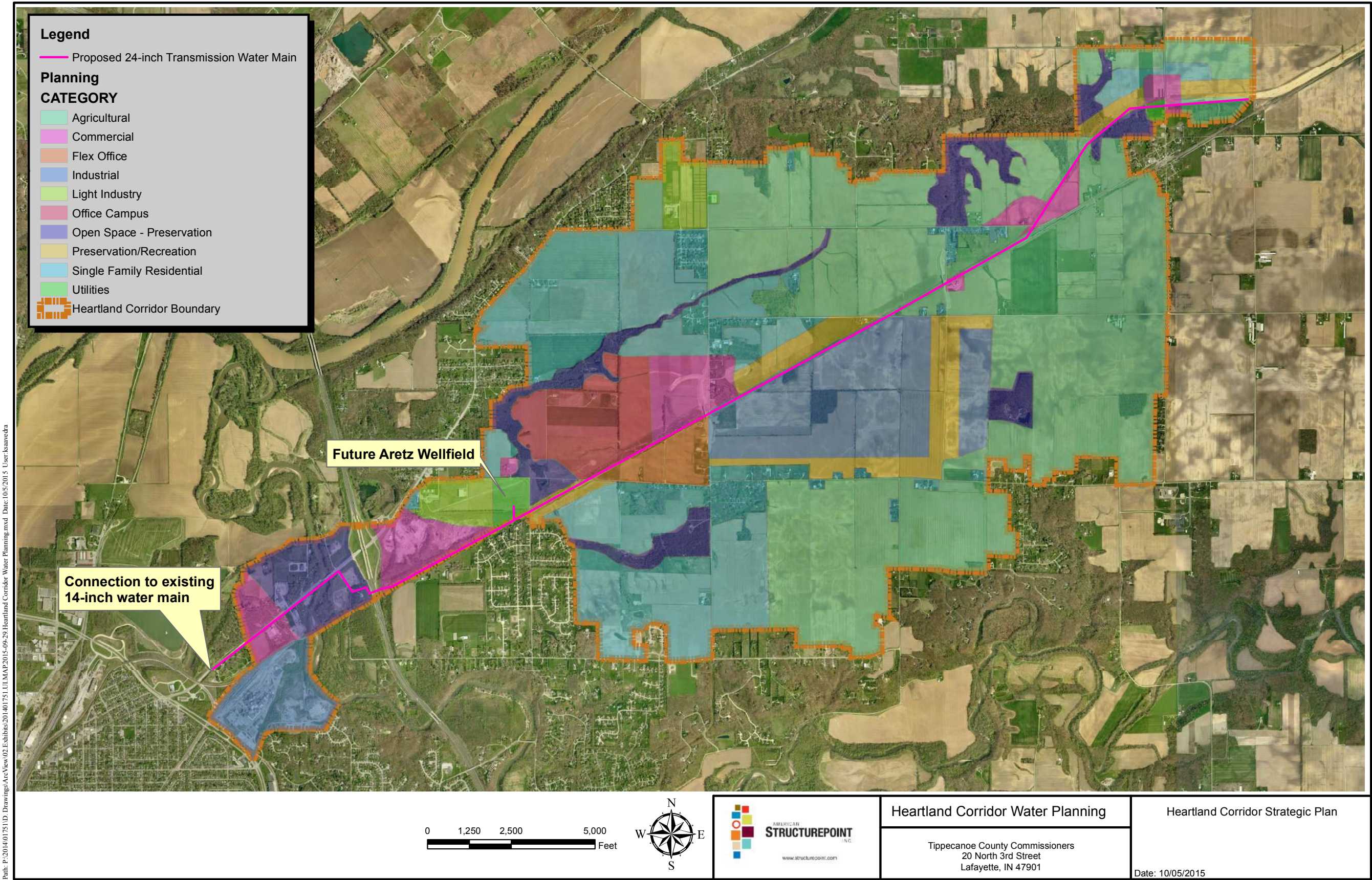
Proposed Land Uses

Single Family Residential	925 Acres
Multi-family Residential	165 Acres
Commercial	195 Acres
Flex Office	70 Acres
Office Campus	110 Acres
Industrial	700 Acres
Preservation/Recreation	30 Acres
Utilities/Government Services	38 Acres
Buffer	

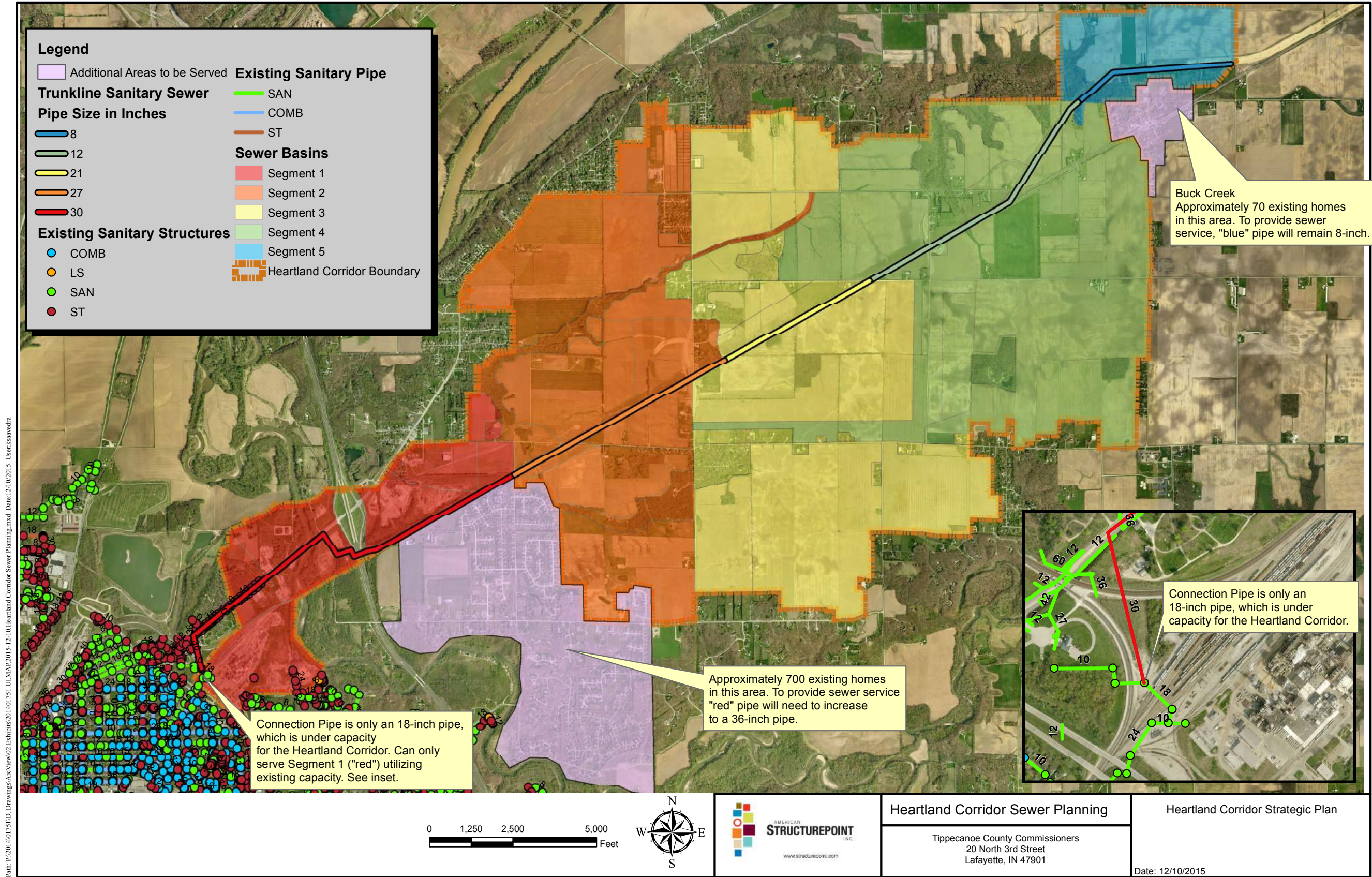
- Streets
- Drainage
- Waterbodies
- Study Boundary

Total Acreage

Proposed Water Infrastructure Map



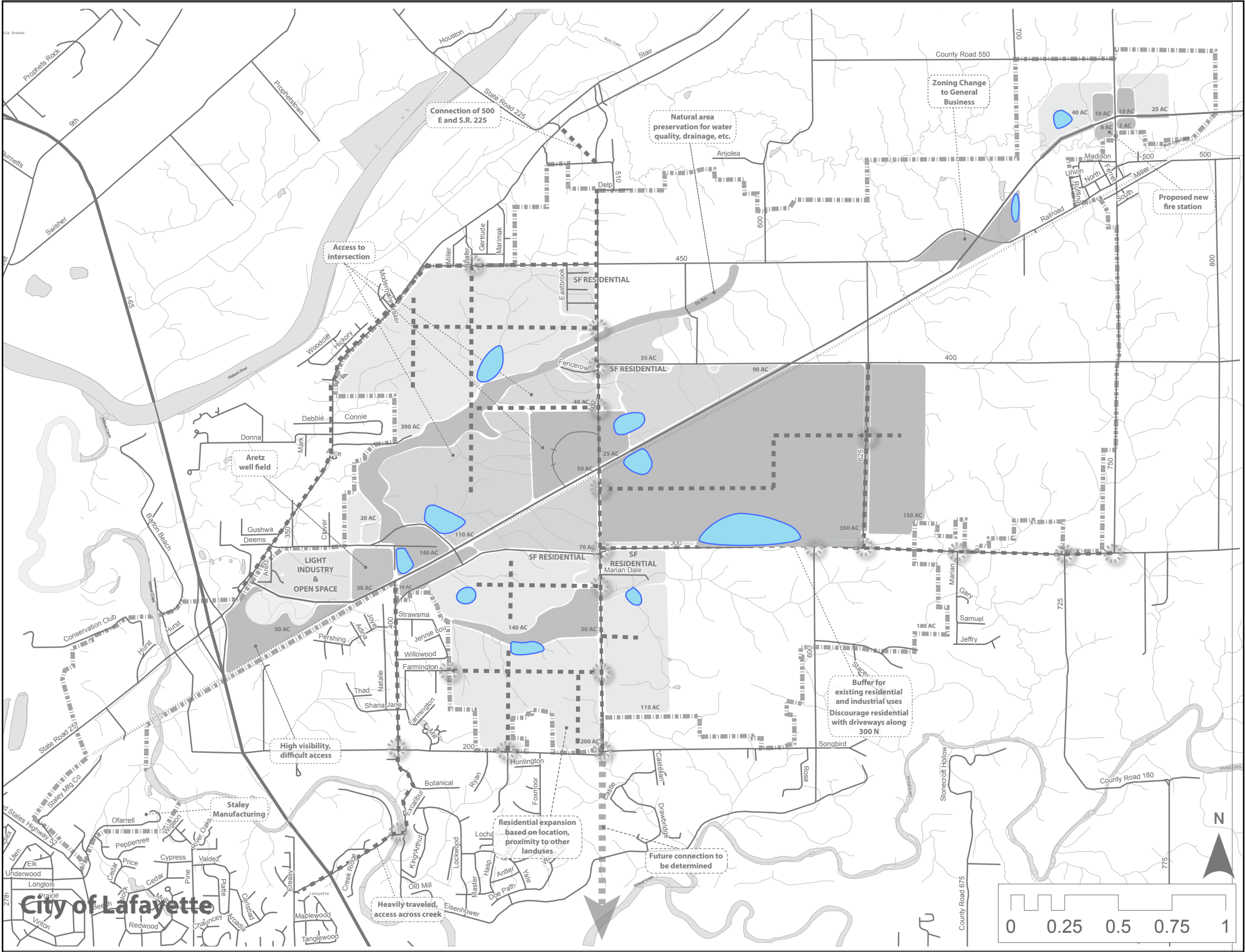
Proposed Sanitary Sewer Infrastructure Map



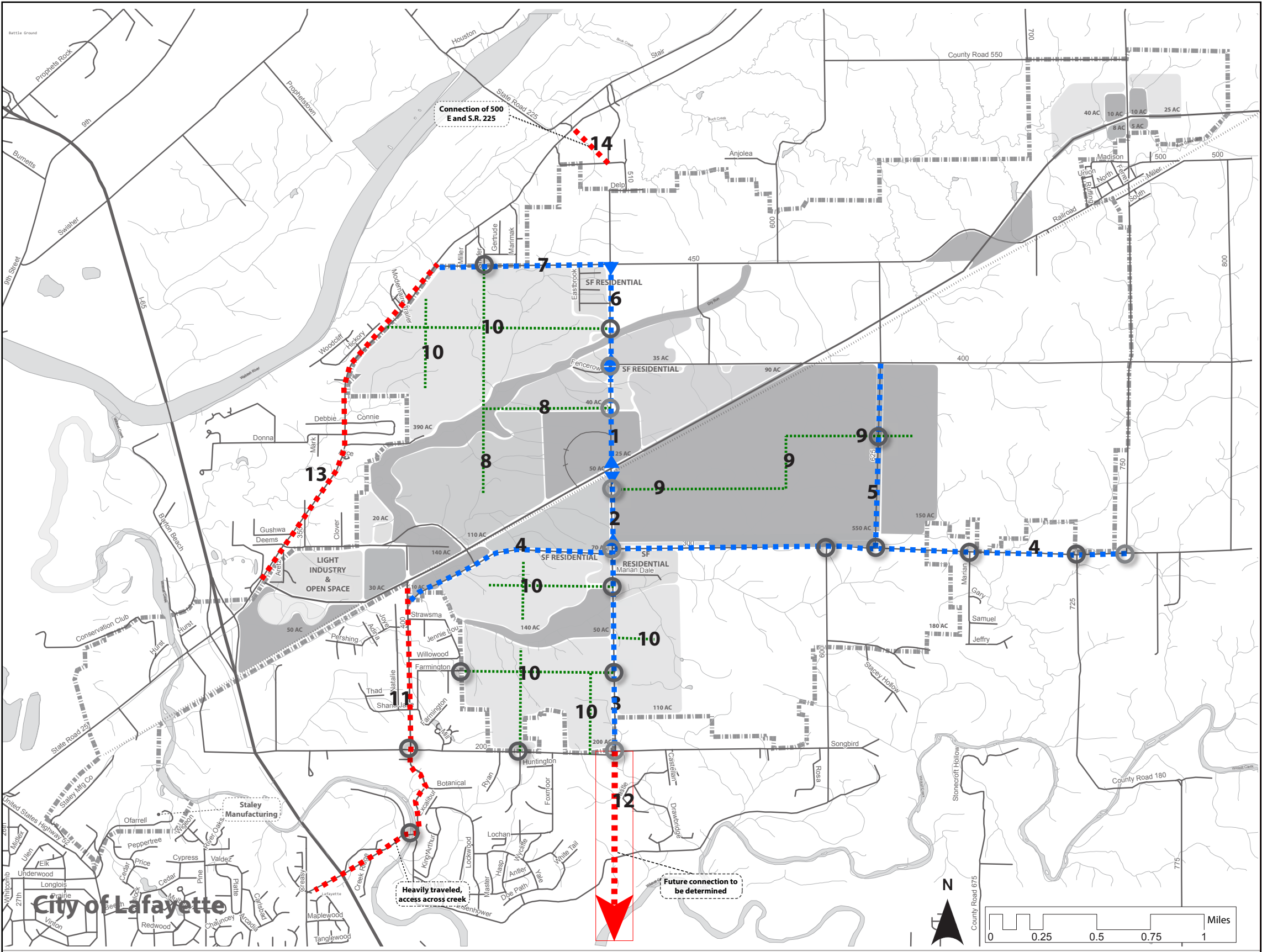
Detention Pond Locations Map

Legend

Detention Pond



Proposed Road Improvements Map



Legend

○ Intersection Improvement

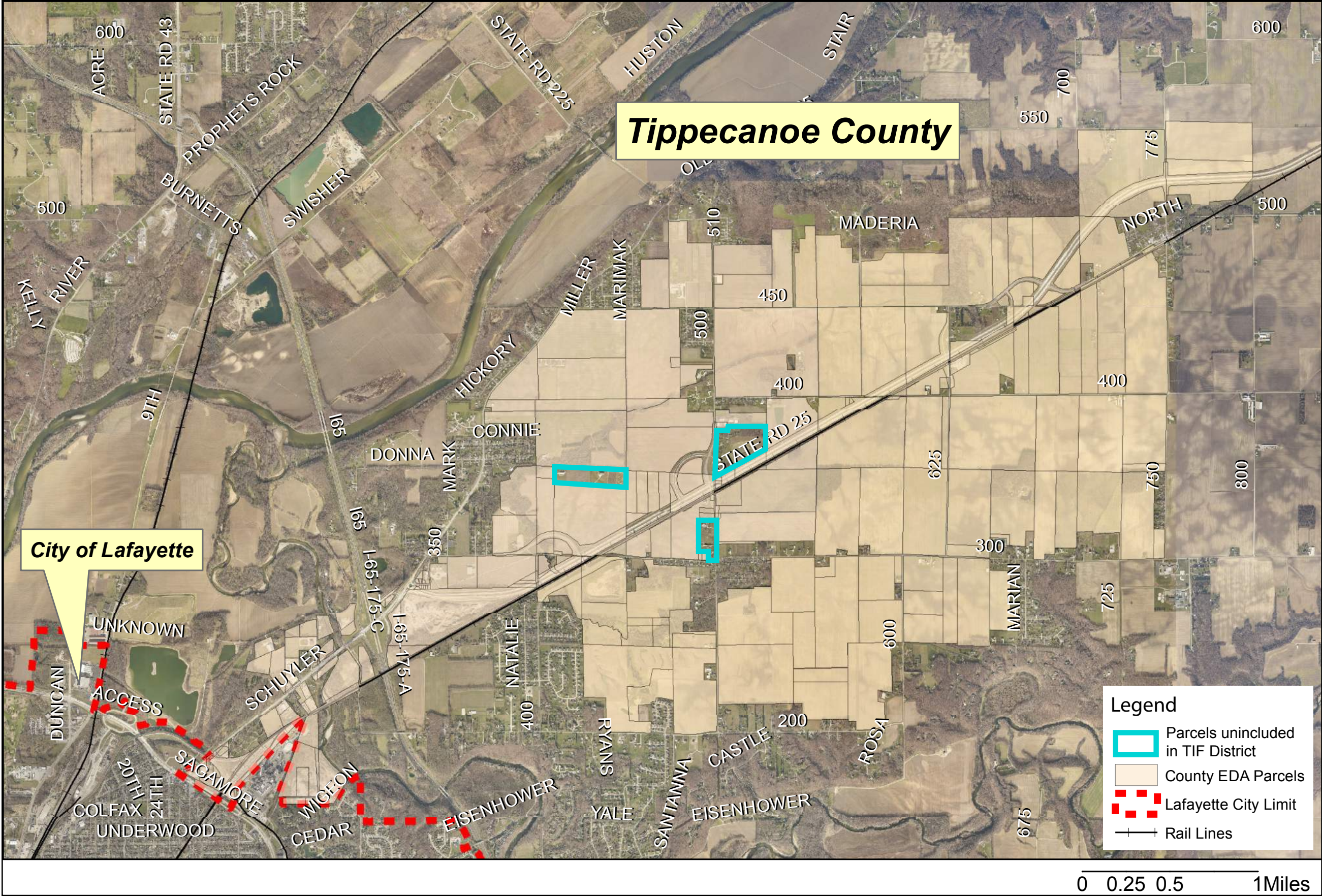
Type of Road Improvement

■ ■ ■ Improvement to Existing Road

■ ■ ■ New Road

■ ■ ■ Corridor and Connectivity Study

TIF Boundary vs. Proposed Land Use Analysis



APPENDIX C

Infrastructure Cost Estimates

Water Infrastructure Cost Estimates

Item No.	Description	Quantity	Unit	Unit Price	Item Total
1	Water Main - 24" Ductile Iron, Open Cut	37,190.00	LFT	\$130.00	\$4,835,000.00
2	Water Main - 30" HDPE Wildcat Creek Crossing, Directional Drill	670.00	LFT	\$160.00	\$107,000.00
3	Water Main - 30" HDPE, Trenchless I-65 Crossing, Directional Drill	240.00	LFT	\$160.00	\$38,000.00
4	Pressure Regulator Valve Vault	1.00	LS	\$40,000.00	\$40,000.00
5	Butterfly Valves - 24", DI (every 1500 feet)	25.00	EA	\$15,000.00	\$375,000.00
6	Maintenance of Traffic (3%)	1.00	LS	\$161,850.00	\$162,000.00
7	Mobilization / Demobilization (NTE 5%)	1.00	LS	\$269,750.00	\$270,000.00

ESTIMATED PROJECT COST
Total: \$5,800,000.00
20.0% Contingency: \$1,200,000.00
ESTIMATED TOTAL: \$7,000,000.00

Sanitary Sewer Infrastructure Cost Estimates

Item No.	Description	Quantity	Unit	Unit Price	Item Total
1	8-inch PVC SDR 26, 12-16' deep, Native soil	3,275.00	LFT	\$88.00	\$288,000.00
2	8-inch PVC SDR 26, 12-16' deep, Granular Backfill	1,638.00	LFT	\$148.00	\$242,000.00
3	12-inch PVC SDR 26, 12-16' deep, Native soil	5,623.00	LFT	\$101.00	\$568,000.00
4	12-inch PVC SDR 26, 12-16' deep, Granular Backfill	2,812.00	LFT	\$167.00	\$470,000.00
5	21-inch RCP, 12-16' deep, Native soil	3,284.00	LFT	\$149.00	\$489,000.00
6	21-inch RCP, 12-16' deep, Granular Backfill	1,642.00	LFT	\$231.00	\$379,000.00
7	27-inch RCP, 12-16' deep, Native soil	4,765.00	LFT	\$196.00	\$934,000.00
8	27-inch RCP, 12-16' deep, Granular Backfill	2,383.00	LFT	\$289.00	\$689,000.00
9	30-inch RCP, 12-16' deep, Native soil	8,511.00	LFT	\$208.00	\$1,770,000.00
10	30-inch RCP, 12-16' deep, Granular Backfill	4,256.00	LFT	\$306.00	\$1,302,000.00
11	42-inch steel casing pipe, Highway crossing Bore	240.00	LFT	\$650.00	\$156,000.00
12	30-inch HDPE, Wildcat Creek crossing, Directional Drill	670.00	LFT	\$160.00	\$107,000.00
13	Duplex submersible lift station, 25-feet deep, 100 gpm	1.00	LS	\$200,000.00	\$200,000.00
14	Duplex submersible lift station, 30-feet deep, 900 gpm	1.00	LS	\$1,200,000.00	\$1,200,000.00
15	4-foot diameter manholes, Native Soil	46.00	EA	\$5,000.00	\$230,000.00
16	5-foot diameter manholes, Native Soil	49.00	EA	\$8,000.00	\$392,000.00
17	42-inch steel casing pipe, Railroad crossing Bore	135.00	LFT	\$760.00	\$103,000.00
18	6-inch DIP Force Main, Native soil	2,000.00	LFT	\$66.00	\$132,000.00
19	8-inch DIP Force Main, Native soil	3,000.00	LFT	\$84.00	\$252,000.00
20	Air Release Valve and Vault	4.00	EA	\$12,500.00	\$50,000.00
21	Maintenance of Traffic (3%)	1.00	LS	\$298,590.00	\$299,000.00
22	Mobilization / Demobilization (NTE 5%)	1.00	LS	\$497,650.00	\$498,000.00

ESTIMATED PROJECT COST
Total: \$10,800,000.00
20.0% Contingency: \$2,200,000.00
ESTIMATED TOTAL: \$13,000,000.00

Road Improvements Cost Estimates

PROJECT DESCRIPTION				PROJECT COSTS										DATA/ASSUMPTIONS									
							10.0%	12.5%							MAJOR			MINOR					
ID	Segment Description	Project Type	Signalized Intersection? (Y/N)	TOTAL	Construction	Design	Inspection	ROW	Signal	Utility Relocation	Property Acquisition	Environmental	Cost per mile	Lanes	Lane Width	Length	Lanes	Lane Width	Length	Earthwork Conflicts	Drainage Conflicts	Structural Conflicts	
01	500E from 400N to Hoosier Heartland Corridor; proposed three lane with curb and gutter, sidewalks separated (5') on both sides, trees, and street lighting. (.43 miles)	Road Improvement		\$ 2,216,258	\$ 1,809,190	\$ 180,919	\$ 226,149	\$ -	\$ -	Not Included	Not Included	Not Included	\$ 3,581,000	3	12	2270				-	-	-	
		Intersection (Tee)	Y	\$ 356,400	\$ 144,000	\$ 14,400	\$ 18,000	\$ -	\$ 180,000								2	10		-	-	-	
		TOTAL		\$ 2,572,658	\$ 1,953,190	\$ 195,319	\$ 244,149																
02	500E from Hoosier Heartland Corridor to 300N; three lane road with curb and gutter, separated multi-use trail (8') on east side, separated sidewalks (5') on west side. (.27 miles)	Road Improvement		\$ 1,442,401	\$ 1,177,470	\$ 117,747	\$ 147,184	\$ -	\$ -	Not Included	Not Included	Not Included	\$ 4,230,000	3	12	1470				-	-	-	
		Intersection (Tee)	Y	\$ 368,650	\$ 154,000	\$ 15,400	\$ 19,250	\$ -	\$ 180,000								2	12		-	-	-	
		Intersection (4-Way)	Y	\$ 617,325	\$ 357,000	\$ 35,700	\$ 44,625	\$ -	\$ 180,000								2	12		-	-	-	
		TOTAL		\$ 2,428,376	\$ 1,688,470	\$ 168,847	\$ 211,059																
03	500E from 300N to 200N; three lane road with curb and gutter, separated multi-use trail (8') on east side, separated sidewalks (5') on west side. (1 mile)	Road Improvement		\$ 5,180,842	\$ 4,229,245	\$ 422,935	\$ 528,662	\$ -	\$ -	Not Included	Not Included	Not Included	\$ 4,230,000	3	12	5280				-	-	-	
		Intersection (4-Way)	Y	\$ 617,325	\$ 357,000	\$ 35,700	\$ 44,625	\$ -	\$ 180,000								2	12		-	-	-	
		TOTAL		\$ 5,798,167	\$ 4,586,245	\$ 458,635	\$ 573,287																
04	300N from 750E to 400E; two lane road with curb and gutter, separated multi-use trail (8') on south side, separated sidewalks (5') on north side. (3.6 miles)	Road Improvement		\$ 11,133,915	\$ 9,088,910	\$ 908,891	\$ 1,136,114	\$ -	\$ -	Not Included	Not Included	Not Included	\$ 3,411,000	2	12	13460				-	-	Box Culvert	
		Intersection (Tee)	N	\$ 170,275	\$ 139,000	\$ 13,900	\$ 17,375	\$ -	\$ -								2	10		-	-	-	
		Intersection (Tee)	N	\$ 170,275	\$ 139,000	\$ 13,900	\$ 17,375	\$ -	\$ -								2	12		-	-	-	
		Intersection (Tee)	N	\$ 170,275	\$ 139,000	\$ 13,900	\$ 17,375	\$ -	\$ -								2	10		-	-	-	
		Intersection (Tee)	N	\$ 170,275	\$ 139,000	\$ 13,900	\$ 17,375	\$ -	\$ -								2	10		-	-	-	
		Intersection (Tee)	Y	\$ 350,275	\$ 139,000	\$ 13,900	\$ 17,375	\$ -	\$ 180,000								2	10		-	-	-	
		TOTAL		\$ 12,165,290	\$ 9,783,910	\$ 978,391	\$ 1,222,989																
05	625E from 400N to 300N; two lane road with curb and gutter, separated multi-use trail (8') on east side, separated sidewalks (5') on west side. (.93 miles)	Road Improvement		\$ 3,855,769	\$ 3,147,566	\$ 314,757	\$ 393,446	\$ -	\$ -	Not Included	Not Included	Not Included	\$ 3,411,000	2	12	4910				-	-	-	
		Intersection (4-Way)	Y	\$ 460,525	\$ 229,000	\$ 22,900	\$ 28,625	\$ -	\$ 180,000								2	10		-	-	-	
		TOTAL		\$ 4,316,294	\$ 3,376,566	\$ 337,657	\$ 422,071																
06	500E from 400N to 450N; three lane road with curb and gutter, separated multi-use trail (8') on east side, separated sidewalks (5') on west side. (.5 miles)	Road Improvement		\$ 2,590,421	\$ 2,114,620	\$ 211,470	\$ 264,331	\$ -	\$ -	Not Included	Not Included	Not Included	\$ 4,230,000	3	12	2640				-	-	Box Culvert	
		Intersection (4-Way)	Y	\$ 617,325	\$ 357,000	\$ 35,700	\$ 44,625	\$ -	\$ 180,000								2	12	-	-	-	-	
		TOTAL		\$ 3,207,746	\$ 2,471,620	\$ 247,170	\$ 308,956																
07	450N from 500E to Old S.R. 25; two lane road with curb and gutter, separated multi-use trail (8') on south side, separated sidewalks (5') on north side. (.87 miles)	Road Improvement		\$ 3,635,145	\$ 2,967,466	\$ 296,747	\$ 370,933	\$ -	\$ -	Not Included	Not Included	Not Included	\$ 3,411,000	2	12	4594				-	-	-	
08	Access to multi-family residential commercial and office park areas; two lane road with curb and gutter, separated sidewalks (5') on both sides.	New Alignment						\$ -	\$ -	Not Included	Not Included	Not Included	\$ 3,295,000	2	12					-	-	-	
		Intersection (Tee)	Y					\$ -	\$ -								4	12		-	-	-	
		TOTAL																					
09	Access to industrial site; two lane road to accommodate heavy truck traffic with curb and gutter and separated sidewalks (5') on both sides.	New Alignment						\$ -	\$ -	Not Included	Not Included	Not Included	\$ 3,295,000	2	12					-	-	-	
		Intersection (4-Way)	N					\$ -	\$ -								2	12		-	-	-	
		TOTAL																					
10	Residential neighborhood network; two lane road with curb and gutter and separated sidewalks (5') on both sides.	New Alignment						\$ -	\$ -	Not Included	Not Included	Not Included	\$ 3,295,000	2	14					-	-	-	
		Intersection (Tee)	N					\$ -	\$ -								4	12		-	-	-	
		Intersection (Tee)	N					\$ -	\$ -								4	12		-	-	-	
		Meet Exist. Pavement	N					\$ -	\$ -								2	14		-	-	-	
		Intersection (Tee)	Y					\$ -	\$ -								2	12					
		Intersection (Tee)	N					\$ -	\$ -								2	12					
		Intersection (Tee)						\$ -	\$ -								4	12		-	-	-	
		Intersection (4-Way)	N					\$ -	\$ -								2	12		-	-	-	
TOTAL																							

PROJECT DESCRIPTION			PROJECT COSTS										DATA/ASSUMPTIONS								
													MAJOR			MINOR					
ID	Segment Description	Project Type	Signalized Intersection? (Y/N)	TOTAL	Construction	Design	Inspection	ROW	Signal	Utility Relocation	Property Acquisition	Environmental	Lanes	Lane Width	Length	Lanes	Lane Width	Length	Earthwork Conflicts	Drainage Conflicts	Structural Conflicts
11*	Eisenhower Road	Road Improvement		\$ 5,000	Not Included	\$ 5,000	Not Included		Not Included	Not Included	Not Included	Not Included	-	-	-				Steep slopes near I-69 and Creek	-	2 Bridges
		Intersection (4-Way)		\$ 5,000		\$ 5,000										-	-		-	-	-
12*	500E (south of 200N)	New Alignment		\$ 5,000		\$ 5,000							4	12	TBD				-	-	Bridge (if crossing Wildcat Creek)
		TBD		\$ 5,000		\$ 5,000													-	-	-
13*	Old S.R. 25 and Schuyler Ave	Road Improvement		\$ 5,000		\$ 5,000							3	12	9780				-	-	-
14*	Old S.R. 25 to 500 E	New Alignment		\$ 5,000		\$ 5,000							4	12	1350				-	-	-
TOTAL				\$ 30,000		\$ 30,000															

*Design Cost includes only scope of work determination

APPENDIX D

Resolutions

RESOLUTION No. 2016-05-AM

A Resolution to Amend the Comprehensive Plan for Tippecanoe County to
Include the Hoosier Heartland Corridor Strategic Economic Development Plan

WHEREAS, the AREA PLAN COMMISSION of TIPPECANOE COUNTY pursuant to IC 36-7-4-511, has adopted the *Hoosier Heartland Corridor Strategic Economic Development Plan* as an amendment to the *Comprehensive Plan for Tippecanoe County* at a public meeting held on January 20, 2016; and

WHEREAS, hearings and the meeting notices required by IC 36-7-4-507 were held, and complied with; and

WHEREAS, a certified copy of the new *Hoosier Heartland Corridor Strategic Economic Development Plan* has been filed with the Auditor of Tippecanoe County, and copies distributed to the Commissioners, and is hereby incorporated by reference and made part of this Resolution;

NOW THEREFORE BE IT RESOLVED BY THE BOARD OF COMMISSIONERS OF TIPPECANOE COUNTY, INDIANA THAT:

THE COMPREHENSIVE PLAN FOR TIPPECANOE COUNTY IS HEREBY AMENDED TO INCLUDE THE HOOSIER HEARTLAND CORRIDOR STRATEGIC ECONOMIC DEVELOPMENT PLAN.

This Resolution shall be in full force and effective from and after its passage.

ADOPTED AND PASSED BY THE BOARD OF COMMISSIONERS OF TIPPECANOE COUNTY, INDIANA, THIS 1st DAY OF February, 2016.

VOTE:





President





Vice President





Member

ATTEST:



Robert Plantenga, Auditor

THE

Area Plan Commission

of TIPPECANOE COUNTY

20 NORTH 3RD STREET
LAFAYETTE, INDIANA 47901-1209

(765) 423-9242
(765) 423-9154 [FAX]
www.tippecanoe.in.gov/apc

SALLIE DELL FAHEY
EXECUTIVE DIRECTOR

January 21, 2016
Ref. No.: 16-007

Tippecanoe County Commissioners
20 N. 3rd Street
Lafayette IN 47901

CERTIFICATION

RE: **COMPREHENSIVE PLAN AMENDMENT
HEARTLAND CORRIDOR STRATEGIC ECONOMIC DEVELOPMENT AND LAND
USE PLAN:**

An amendment to the Comprehensive Plan that includes the land surrounding the Hoosier Heartland Highway from I-65 to the Tippecanoe County line

Dear Commissioners:

As Secretary to the Area Plan Commission of Tippecanoe County, I do hereby certify that at a public hearing held January 20, 2016, the Area Plan Commission of Tippecanoe County, by a vote of 13 yes – 0 no, approved **Resolution 2016-01**, adopting the *Heartland Corridor Strategic Economic Development and Land Use Plan* as an amendment to the Comprehensive Plan for Tippecanoe County, and thus recommended it for APPROVAL by the Tippecanoe County Commissioners.

Enclosed is a copy of the Plan with our staff report as certified by me to the Lafayette Common Council and the resolution to be adopted by the Lafayette Common Council.

For the Tippecanoe County Commissioners to adopt this Comprehensive Plan amendment, it will need to pass the enclosed Resolution and return it to me. At that time I will file the signed Resolution and the plan amendment with the County Recorder as per statute.

If there are questions concerning the above, please contact me.

Sincerely,


Sallie Dell Fahey
Executive Director

Enclosures (3)

cc: Dave Luhman

RESOLUTION NO. 2016-01

WHEREAS, the *Comprehensive Plan* was adopted in 1981 and allows for the inclusion of economic development and land use plans; and

WHEREAS, the Area Plan Commission of Tippecanoe County derives authority to approve amendments to the adopted *Comprehensive Plan* from *Indiana Code Section 36-7-4-511*; and


WHEREAS, the Area Plan Commission of Tippecanoe County did hold a public hearing following proper publication of meeting notices under *Indiana Code Section 36-7-4-507*, to inform and hear discussion on this amendment to the adopted *Comprehensive Plan*; and

WHEREAS, the Area Plan Commission of Tippecanoe County after due consideration, adopted this amendment known as the *Hoosier Heartland Corridor Strategic Economic Development Plan*, an amendment to the adopted *Comprehensive Plan*, finding it to be in accord with its own goals, thus recommending the amendment to Tippecanoe County and the City of Lafayette;

NOW THEREFORE, BE IT RESOLVED BY THE AREA PLAN COMMISSION OF TIPPECANOE COUNTY, THAT:

The *Hoosier Heartland Corridor Strategic Economic Development Plan*, a part of the *Comprehensive Plan for Tippecanoe County*, is hereby adopted.

This Resolution shall be in full force and effect from and after its passage.



Gerry Keen, President

Attest:



Sallie Dell Fahey, Secretary

Tom Murtaugh
David Byers
Tracy Brown

County Office Building
20 North 3rd Street
Lafayette, Indiana
47901-1214

phone 765.423.9215
fax 765.423.9196

January 20, 2016

TIPPECANOE COUNTY BOARD OF COMMISSIONERS

Dear Plan Commission Members, Council Members, and Commissioners:

The Heartland Corridor Strategic Economic Development and Land Use Plan Steering Committee, consisting of 20 community members of varied backgrounds and interests, has been working with American Structurepoint since April 2015 to develop an economic development and land use plan for the Heartland Corridor TIF area that provides a guide to improve, develop and redevelop this area of the county over the next several decades. After five steering committee meetings, two days of focus group meetings, one general public meeting, one online review period, three informational meetings, countless email communications, hundreds of postcard mailings, and many draft documents, we now have a plan that is before you for your consideration. This plan is forward thinking, providing objectives and strategies for decision makers, organizations, and citizens throughout Tippecanoe County to improve and grow our community over the next 20 years and beyond.

Over the last two months, each committee member has had the opportunity to provide comments and recommendations on the draft plan, and each of those ideas has been incorporated into the plan in some fashion. Subsequently, the Steering Committee is in **full support** of the draft plan before you tonight and encourages each advisory or legislative body to recommend favorably for and/or adopt this plan as required.

On December 2, 2015, the Hoosier Heartland Corridor Strategic Economic Development and Land Use Plan was introduced to the Lafayette City Council and reviewed by the Ordinance Committee and forwarded to the Area Plan Commission. On January 13, 2016, the Hoosier Heartland Corridor Strategic Economic Development and Land Use Plan was approved by the Redevelopment Commission and forwarded to the County Commissioners.

As with any plan of this nature, we need to continue to meet periodically to evaluate the goals and strategies, review progress, and make changes as needed. We look forward to the opportunity to implement our plan in Tippecanoe County.

Thank you to American Structurepoint for their hard work and a job well done.

Sincerely,



Tom Murtaugh, Tippecanoe County Commissioner
Heartland Corridor Steering Committee Chairperson

FILED
CITY CLERK

2016 JAN 22 A 11:44

RESOLUTION No. 2016-02

CINDY MURRAY

**A Resolution to Amend the Comprehensive Plan for Tippecanoe County to
Include the Hoosier Heartland Corridor Strategic Economic Development Plan**

WHEREAS, the AREA PLAN COMMISSION of TIPPECANOE COUNTY pursuant to IC 36-7-4-511, has adopted the *Hoosier Heartland Corridor Strategic Economic Development Plan* as an amendment to the *Comprehensive Plan for Tippecanoe County* at a public meeting held on January 20, 2016 to replace the 1981 Housing Element; and

WHEREAS, hearings and the meeting notices required by IC 36-7-4-507 were held, and complied with; and

WHEREAS, a certified copy of the *Hoosier Heartland Corridor Strategic Economic Development Plan* has been filed with the Clerk Treasurer of the City of Lafayette, and copies distributed to the members of the Council, and is hereby incorporated by reference and made part of this Resolution;

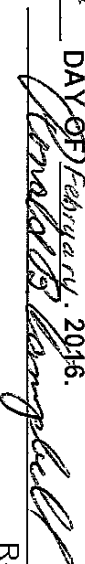
NOW THEREFORE BE IT RESOLVED BY THE COUNCIL OF THE CITY OF LAFAYETTE, INDIANA THAT:

THE COMPREHENSIVE PLAN FOR TIPPECANOE COUNTY IS HEREBY AMENDED TO INCLUDE THE HOOSIER HEARTLAND CORRIDOR STRATEGIC ECONOMIC DEVELOPMENT PLAN.

This Resolution shall be in full force and effective from and after its passage.

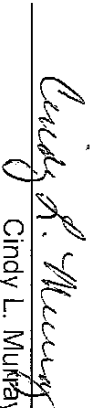
ADOPTED AND PASSED BY THE COUNCIL OF THE CITY OF LAFAYETTE, INDIANA, THIS

1st DAY OF February, 2016.



Ronald B. Campbell, Presiding Officer

ATTEST:



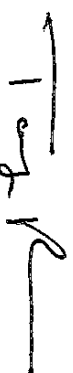
Cindy L. Murray, City Clerk

Presented by me to the Mayor of the City of Lafayette, Indiana, on the 1st day of February, 2016.

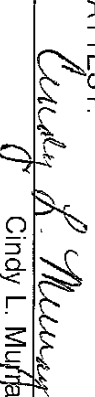


Cindy L. Murray, City Clerk

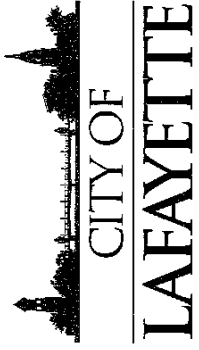
This Resolution approved and signed by me on the 1st day of February, 2016.



ATTEST:



Cindy L. Murray, City Clerk



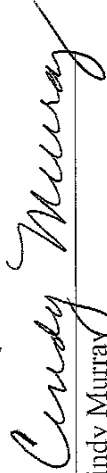
CINDY MURRAY
CITY CLERK

Certificate

STATE OF INDIANA)
)
COUNTY OF TIPPECANOE) SS:
)
CITY OF LAFAYETTE)

This is to certify that the Common Council of the City of Lafayette did pass Resolution 2016-02 at their regular meeting on the 1st day of February 2016. The vote was 8 to 0.

WITNESS MY HAND AND THE SEAL OF THE CITY OF LAFAYETTE, INDIANA ON
THIS 1st DAY OF FEBRUARY 2016.


Cindy Murray
City Clerk, City of Lafayette

SEAL

THE
Area Plan Commission
of TIPPECANOE COUNTY

20 NORTH 3RD STREET
LAFAYETTE, INDIANA 47901-1209

(765) 423-9242
(765) 423-9154 [FAX]
www.tippecanoe.in.gov/apc

SALLIE DELL FAHEY
EXECUTIVE DIRECTOR

January 21, 2016
Ref. No.: 16-008

Lafayette Common Council
20 N. 6th Street
Lafayette IN 47901

CERTIFICATION

RE: **COMPREHENSIVE PLAN AMENDMENT
HEARTLAND CORRIDOR STRATEGIC ECONOMIC DEVELOPMENT AND LAND
USE PLAN:**

An amendment to the Comprehensive Plan that includes the land surrounding the
Hoosier Heartland Highway from I-65 to the Tippecanoe County line

Dear Council:

As Secretary to the Area Plan Commission of Tippecanoe County, I do hereby certify that at a public hearing held January 20, 2016, the Area Plan Commission of Tippecanoe County, by a vote of 13 yes – 0 no, approved **Resolution 2016-01**, adopting the *Heartland Corridor Strategic Economic Development and Land Use Plan* as an amendment to the Comprehensive Plan for Tippecanoe County, and thus recommended it for APPROVAL by the Lafayette Common Council.

Enclosed is a copy of the Plan with our staff report as certified by me to the Lafayette Common Council and the resolution to be adopted by the Lafayette Common Council.

For the Lafayette Common Council to adopt this Comprehensive Plan amendment, it will need to pass the enclosed Resolution and return it to me. At that time I will file the signed Resolution and the plan amendment with the County Recorder as per statute.

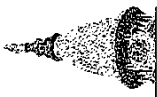
If there are questions concerning the above, please contact me.

Sincerely,


Sallie Dell Fahey
Executive Director

Enclosures (3)

cc: Edward Chosnek



Tom Murtaugh
David Byers
Tracy Brown

TIPPECANOE COUNTY BOARD OF COMMISSIONERS

County Office Building
20 North 3rd Street
Lafayette, Indiana
47901-1214

January 20, 2016

phone 765.423.9215
fax 765.423.9196

Dear Plan Commission Members, Council Members, and Commissioners:

The Heartland Corridor Strategic Economic Development and Land Use Plan Steering Committee, consisting of 20 community members of varied backgrounds and interests, has been working with American Structurepoint since April 2015 to develop an economic development and land use plan for the Heartland Corridor TIF area that provides a guide to improve, develop and redevelop this area of the county over the next several decades. After five steering committee meetings, two days of focus group meetings, one general public meeting, one online review period, three informational meetings, countless email communications, hundreds of postcard mailings, and many draft documents, we now have a plan that is before you for your consideration. This plan is forward thinking, providing objectives and strategies for decision makers, organizations, and citizens throughout Tippecanoe County to improve and grow our community over the next 20 years and beyond.

Over the last two months, each committee member has had the opportunity to provide comments and recommendations on the draft plan, and each of those ideas has been incorporated into the plan in some fashion. Subsequently, the Steering Committee is in **full support** of the draft plan before you tonight and encourages each advisory or legislative body to recommend favorably for and/or adopt this plan as required.

On December 2, 2015, the Hoosier Heartland Corridor Strategic Economic Development and Land Use Plan was introduced to the Lafayette City Council and reviewed by the Ordinance Committee and forwarded to the Area Plan Commission. On January 13, 2016, the Hoosier Heartland Corridor Strategic Economic Development and Land Use Plan was approved by the Redevelopment Commission and forwarded to the County Commissioners.

As with any plan of this nature, we need to continue to meet periodically to evaluate the goals and strategies, review progress, and make changes as needed. We look forward to the opportunity to implement our plan in Tippecanoe County.

Thank you to American Structurepoint for their hard work and a job well done.

Sincerely,

Tom Murtaugh, Tippecanoe County Commissioner
Heartland Corridor Steering Committee Chairperson

RESOLUTION NO. 2016-01

WHEREAS, the *Comprehensive Plan* was adopted in 1981 and allows for the inclusion of economic development and land use plans; and

WHEREAS, the Area Plan Commission of Tippecanoe County derives authority to approve amendments to the adopted *Comprehensive Plan* from *Indiana Code Section 36-7-4-511*; and


WHEREAS, the Area Plan Commission of Tippecanoe County did hold a public hearing following proper publication of meeting notices under *Indiana Code Section 36-7-4-507*, to inform and hear discussion on this amendment to the adopted *Comprehensive Plan*; and

WHEREAS, the Area Plan Commission of Tippecanoe County after due consideration, adopted this amendment known as the *Hoosier Heartland Corridor Strategic Economic Development Plan*, an amendment to the adopted *Comprehensive Plan*, finding it to be in accord with its own goals, thus recommending the amendment to Tippecanoe County and the City of Lafayette;

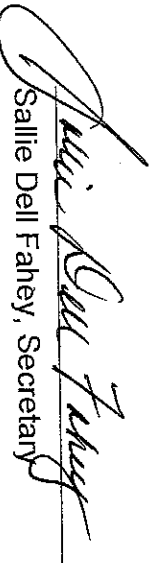
NOW THEREFORE, BE IT RESOLVED BY THE AREA PLAN COMMISSION OF TIPPECANOE COUNTY, THAT:

The *Hoosier Heartland Corridor Strategic Economic Development Plan*, a part of the *Comprehensive Plan for Tippecanoe County*, is hereby adopted.

This Resolution shall be in full force and effect from and after its passage.


Gerry Keen, President

Attest


Sallie Dell Fahey, Secretary

**RESOLUTION 2016-01
COMPREHENSIVE PLAN AMENDMENT
HOOSIER HEARTLAND CORRIDOR STRATEGIC ECONOMIC
DEVELOPMENT PLAN**

**STAFF REPORT
January 14, 2016**

RESOLUTION 2016-01 Comprehensive Plan Amendment Hoosier Heartland Corridor Strategic Economic Development Plan

Staff Report
January 14, 2016

Plan is available for viewing at: <http://www.heartlandstrategicplan.com/>

On November 3, 2014 the Common Council of the City of Lafayette adopted Resolution 2014-19 to approve the creation of an Economic Development Plan for the Lafayette Heartland Economic Development Area. On November 17, 2014 the Tippecanoe County Commissioners adopted Resolution 2014-29-CM establishing the Tippecanoe County Heartland Economic Development Area and on December 1, 2014 approved an agreement for a consultant to lead the development of a formal Economic Development Plan and Land Use Plan for the Hoosier Heartland Corridor.

In January of 2015 a steering committee was organized by the consultant and comprised of APC staff, city staff and a variety of stakeholders including homeowners, business owners and others. Its first meeting took place in April of 2015. The principle task of the steering committee was to oversee and guide the consultant's work and assist in the outreach to the community. In June of 2015 an open house event was held at the Buck Creek Community Center to formally engage the public on their thoughts about the development future of the new Hoosier Heartland Corridor. All property owners within and adjacent to the study area were notified via mail. The event turnout was high and many constructive comments were incorporated into the draft, particularly the proposed land use plan map. The formal comment period that began with the open house event formally closed late in the fall.

The resulting land use plan, which can only materialize once public sanitary sewer becomes available, is designed to strike a balance between achieving the highest and best use for land along valuable transportation networks with the dominant and highly productive agricultural areas in this part of the county. The result is a plan that tightly focuses development opportunities at principle intersections (primarily the I-65 interchange area and Hoosier Heartland's intersection with CR 500 E) while leaving the remaining land in the study area largely unchanged.

In all, this plan sets the stage for the responsible development of the Hoosier Heartland Corridor once the community is ready to address the issue of public sewer. Staff recommends adoption of the Hoosier Heartland Corridor Strategic Economic Development Plan and its inclusion in the *Comprehensive Plan of Tippecanoe County*.

RECOMMENDATION:
Approval